

Nebraska Truck Information Guide



**Nebraska State Patrol
Carrier Enforcement
2013-2014**

INTRODUCTION

In order to make your travels into and through our state safer and less complicated, the Nebraska State Patrol Carrier Enforcement Division is pleased to provide this information guide which summarizes many of the rules and regulations concerning the operation of any freight carrying vehicle, truck, truck-tractor or trailer and passenger carrying bus. This booklet is a guide to help you travel legally in Nebraska. However, it does not contain every rule; if you need more detailed information please contact the agencies shown on page 2.

Disclaimer

*This manual is intended for information only.
It is not a thorough listing of all applicable
state statutes and is not intended to be a
substitute for applicable state statutes.*

Welcome to Nebraska
402-471-4545



Carrier Enforcement
402-471-0105
Fax 402-471-3295

www.statepatrol.nebraska.gov/carriierenforcement.aspx



Pro Bono Publico

"for the good of the public"



Dear Motor Carriers and Agricultural Operators:

Welcome to the 2013-2014 edition of the Nebraska Truck Information Guide. This guide summarizes many of the state laws (weights and measures, permits, registrations, etc.) and Federal Motor Carrier Safety Regulations (CDL requirements, hours of service limitations, vehicle maintenance and inspections, etc.) that regulate the operation of commercial and agricultural vehicles within the state of Nebraska.

The Nebraska State Patrol continually strives to reduce the number of commercial motor vehicle related collisions and fatalities. There is no commodity more valuable than your life, and the lives of your family, friends, and fellow citizens. Please utilize this guide book and the educational and technical assistance it provides to its fullest potential.

Join us in making Nebraska the safest place to travel by obeying all traffic laws, practicing safe driving techniques and keeping your vehicle in optimal operating condition.

Sincerely,

David A. Sankey, Colonel
Superintendent of Law Enforcement and Public Safety

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Nebraska State Patrol Offices

State Headquarters

PO Box 94907
1600 Highway 2
Lincoln, NE 68509
(402) 471-4545

Carrier Enforcement

3920 W Kearney St.
Lincoln, NE 68524
(402) 471-0105
Fax (402) 471-3295

Nebraska State Patrol

www.statepatrol.nebraska.gov/carrierenforcement.aspx

Emergency Help Line **only** 1-800-525-5555

Or * 55 on cellular phone

Troop Area Headquarters

TROOP H

4130 NW 37th St.
Lincoln, NE 68524
(402) 471-4680

TROOP C

3431 W Old Potash Hwy.
Grand Island, NE 68803
(308) 385-6000

TROOP A

4411 S 108th St.
Omaha, NE 68137
(402) 331-3333

TROOP D

300 W South River Rd.
North Platte, NE 69101
(308) 535-8047

TROOP B

1401 W Eisenhower Ave.
Norfolk, NE 68701
(402) 370-3456

TROOP E

4500 Avenue I
Scottsbluff, NE 69361
(308) 632-1211

Nebraska Road Conditions

Dial 511

or

www.511.nebraska.gov

AGENCY	CONTACT INFORMATION	SERVICES PROVIDED
Federal Motor Carrier Safety Administration (FMCSA)	Federal Office 1200 New Jersey Ave SE Washington, DC 20590 800-832-5660 www.fmcsa.dot.gov	Interstate USDOT Number, Data Qs New Entrant Program/Compliance Reviews Operating Authority, Pre-Employment Screenings Federal Motor Carrier Safety Regulations Hazardous Materials Regulations & Safety Permits
Safety & Fitness Electronic Records System (SAFER)	www.safer.fmcsa.dot.gov	Company Snapshot (free) Company Profile (fees assessed)
Compliance, Safety, Accountability (CSA)	csa.fmcsa.dot.gov	CSA Information
Internal Revenue Service (IRS)	Offices in: Lincoln 402-473-4680 Norfolk 402-371-1503 North Platte 308-532-8810 www.irs.gov	Federal Heavy Vehicle Use Tax (Form 2290) Dyed Fuel Information
Pipeline and Hazardous Materials Safety Administration (PHMSA)	East Building, 2nd Floor 1200 New Jersey Ave., SE Washington, DC 20590 202-366-4433 phmsa.dot.gov/hazmat	Hazardous Materials Regulations Hazardous Materials Registrations Hazardous Materials Special Permits
Transportation Security Administration (TSA)	601 S 12th Street Arlington, VA 20598 866-289-9673 www.tsa.gov	Transportation Worker Identification Card (TWIC) Hazardous Materials Endorsement Threat Assessment Program (HTAP) Fingerprinting locations www.hazprints.com

AGENCY	CONTACT INFORMATION	SERVICES PROVIDED
Nebraska State Patrol Carrier Enforcement Division	3920 W Kearney St Lincoln, NE 68524 402-471-0105 Fax 402-471-3295 www.statepatrol.nebraska.gov/carriert enforcement.aspx	Prorate & Fuel Permit Information Dyed Fuel Information Hazardous Material & Safety Regulations Intrastate USDOT Numbers New Entrant Program/Compliance Reviews
Nebraska Department of Motor Vehicles Driver Licensing Services	301 Centennial Mall South – PO Box 94726 Lincoln, NE 68509 402-471-3861 www.dmv.ne.gov	Commercial Driver's Licensing (CDL) Link to Hazmat Fingerprint Locations Online License/Registration Renewal www.clickdmv.ne.gov
Nebraska Department of Motor Vehicles Division of Motor Carrier Services	301 Centennial Mall South – PO Box 94729 Lincoln, NE 68509 Toll Free - 888-622-1222 Local – 402-471-4435 www.dmv.ne.gov/mcs	International Registration Plan (IRP) International Fuel Tax Agreement (IFTA) Unified Carrier Registration (UCR)
Nebraska Department of Revenue Taxpayer Assistance Office	301 Centennial Mall South – PO Box 98904 Lincoln, NE 68509 Toll Free - 800-554-3835 Local – 402-471-5730 www.revenue.ne.gov/fuels	Motor Fuel Tax - Non IFTA Related Questions Dyed Fuel Information
Nebraska Department of Roads	1500 Highway 2 – PO Box 94759 Lincoln, NE 68509 402-471-4567 Fax – 402-471-4325 www.nebraskatransportation.org	Road Closures 511 – Weather Conditions Nebraska Maps Road Construction
Nebraska Department of Roads Permit Office	501 S. 14th St Lincoln, NE 68512 402-471-0034 www.truckpermits.nebraska.gov	Overdimensional/Overweight Permits Conditional Interstate Use Permits
Nebraska Public Service Commission	1200 N Street Suite 300 Lincoln, NE 68508 402-471-3101 800-526-0017 (Toll-free NE Only) www.psc.state.ne.us	Operating Authority for Carriers of Passengers and Household Goods (Intrastate Only)

FREQUENTLY ASKED QUESTIONS

Which vehicles need to stop at weigh stations?

All pickup trucks with a factory-rated capacity of more than one ton, pickup trucks pulling trailers, truck-tractors, and trucks must stop at weigh stations. This includes rental trucks such as Ryder, Penske, or U-Haul, whether or not they are involved in commerce. Recreational vehicles are exempt.

I have a CDL; does the 0.04 blood alcohol concentration (BAC) limit apply when I am driving my personal vehicle?

No. See page 11 for information regarding CDL disqualification.

What is the maximum length for a pickup pulling a camper and boat?

65 feet is the maximum overall length allowed for this combination. See page 19 for more information.

Does a farmer need a DOT number to cross state lines?

Yes, if the vehicle **or combination of vehicles** has a gross vehicle weight or gross vehicle weight rating over 10,000 pounds **and is being used in the furtherance of farm business (such as picking up machinery, delivering crops to the elevator, etc.)**. See page 35 for more information.

Are there any Safe Havens in Nebraska?

Yes. The Village of Hershey has designated Tomahawk Oil Company LTD Truck Stop at Exit 164 off I-80 as a safe haven.

Are there any preferred routes for the transportation of Hazardous Materials in Nebraska?

Yes. The preferred route through Nebraska is Interstate 80, with a detour on I-680 around Omaha. If driving to or through Lincoln, NE from the southern part of the state, Highway 2 is preferred.

Are state permits required to transport hazardous materials, radioactive materials, or hazardous wastes?

No. If you comply with the U.S. DOT and EPA regulations, you will be in compliance with Nebraska's statutes.

I am transporting a load of baled livestock forage (e.g., hay) which is more than 8'6" wide. Can I transport this load of hay on the Interstate system?

No, you cannot. Baled livestock forage is granted a width exception during daylight hours only, up to 12' wide, for all roads *except* for the Interstate system. These loads are not eligible for over-width permits. (See page 22.)

Does my trailer need brake lights and turn signals?

Any trailer in use on a highway must be equipped with brake and turn signal lights in good working order. During daylight hours, fertilizer trailers as defined in State Statute 60-326 and implements of husbandry designed primarily or exclusively for use in agricultural operations are not required to have or maintain brake or turn signal lights, but they may be equipped if desired.

Does my trailer need brakes and/or safety chains?

See page 34.

Our company safety profile shows an inspection/accident report that does not belong to us. How do I get it removed from our profile?

You must file a challenge through the federal DataQ system at: <https://dataqs.fmcsa.dot.gov>. Please note that this system can only be used for challenging inspection/accident data, not for citations or warnings.

Additionally, the DataQ system is used by motor carriers and drivers to challenge any potentially incorrect or incomplete information shown on inspection and/or accident reports.

I have a concealed carry handgun permit that is honored by the state of Nebraska. Can I carry my handgun in my truck?

Yes, you can, provided you follow all rules that are a condition of having a concealed carry handgun permit. See the Nebraska State Patrol Concealed Carry webpage at www.statepatrol.nebraska.gov/ConcealedCarry.aspx for more details.

I have PrePass and am driving toward an open weigh station in Nebraska. Do I follow my in-cab signal or do I obey the roadside signs that tell me to exit or bypass?

In Nebraska, unless you are operating under an overdimensional or overweight permit, you should always follow your in-cab signal and ignore the roadside signs. When operating under an overdimensional or overweight permit you must always exit to an open weigh station, no matter what the in-cab signal or roadside signs instruct you to do; see page 53 for more PrePass information.

When is Nebraska's planting and harvesting season?

February 15 to December 15.

Who can conduct the required annual vehicle inspections? Can I conduct annual inspections for my own vehicles?

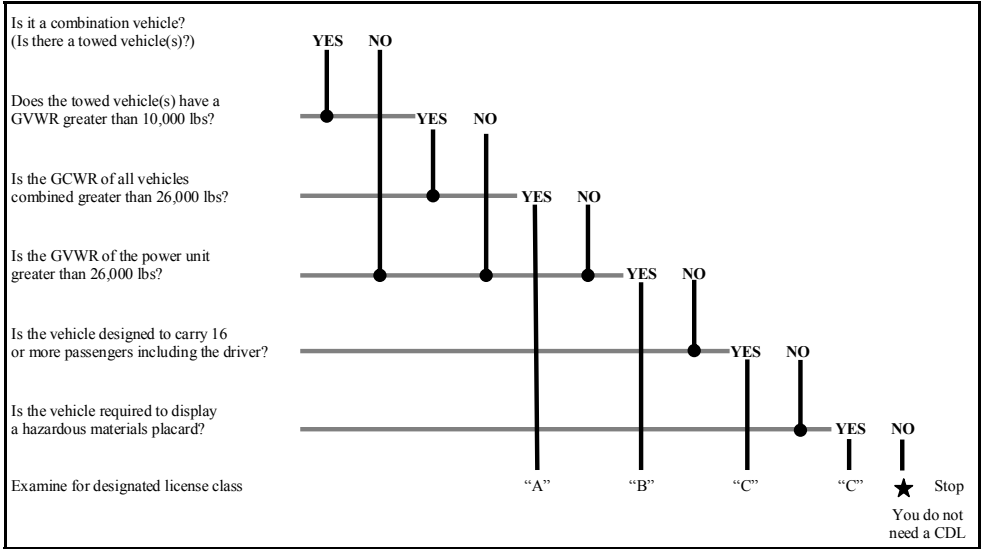
Any qualified person can conduct annual vehicle inspections. See 49 CFR parts 396.19 and 396.25 of the Federal Motor Carrier Safety Regulations for inspector and brake inspector qualifications.

Do I need a medical certificate (DOT physical) to operate a non-CDL commercial motor vehicle? A CDL-requiring commercial motor vehicle? A farm plated vehicle?

See page 42 for medical certificate information and page 35 for farm regulations.

NEBRASKA COMMERCIAL DRIVER'S LICENSE

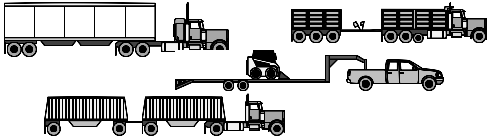
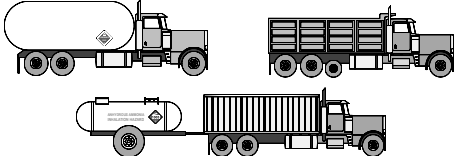
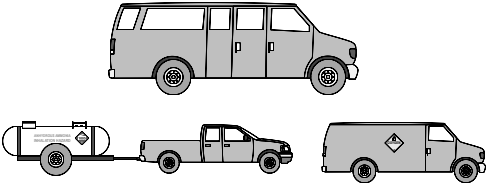


A commercial driver's license (CDL) shall mean an operator's license issued in accordance with the requirements of the Motor Vehicle Operator's License Act to an individual which authorizes such individual to drive a class of commercial motor vehicle. To determine which class CDL is required, follow the flow chart below.



Gross vehicle weight rating (GVWR) means the value specified by the manufacturer as the maximum loaded weight of a single motor vehicle. The gross combination weight rating (GCWR) of a vehicle shall be the GVWR of the power unit plus the GVWR of the towed unit or units. In the absence of a value specified for the towed unit or units by the manufacturer, the GVWR of a combination or articulated vehicle shall be the GVWR of the power unit plus the total weight of the towed unit or units and the loads on such towed unit or units. The GVWR for multiple towed units are added to determine whether the 10,000 pound GVWR threshold has been met.

VEHICLES EXEMPT FROM CDL:

1. Farm-plated trucks, except for farm-plated tractor trailers, within 150 miles of the owner's farm or ranch. ***(If crossing state lines, check with neighboring states.)***
2. Any recreational vehicle as defined in section 60-347 or motor vehicle towing a cabin trailer as defined in sections 60-314 and 60-339.
3. Emergency vehicles operated by a public or volunteer fire department.
4. Military vehicles driven by active-duty military personnel.
5. Vehicles driven by members of the military reserves, other than military technicians.
6. Vehicles driven by active-duty Coast Guard personnel.
7. Vehicles driven by members of the National Guard on active-duty including:
 - a) Personnel on full-time National Guard duty;
 - b) Personnel on part-time National Guard training; and
 - c) National Guard Military Technicians required to wear military uniforms.

Class	Description	Examples
A	<ul style="list-style-type: none"> • Combination vehicle • GCWR over 26,000 lbs. • Towed unit(s) GVWR over 10,000 lbs. • Includes class B, C and O vehicles 	
B	<ul style="list-style-type: none"> • Single vehicle with a GVWR over 26,000 lbs. • Towed unit GVWR not exceeding 10,000 lbs. • Includes class C and O vehicles 	
C	<ul style="list-style-type: none"> • Single vehicle with a GVWR less than 26,001 lbs. • Towed unit GVWR not exceeding 10,000 lbs. • Designed to transport 16 or more persons including driver • Hauling hazmat which requires placards 	
O	<ul style="list-style-type: none"> • Any non-commercial vehicle except motorcycle. 	
M	<ul style="list-style-type: none"> • Any motorcycle 	

Age Intrastate (within Nebraska) Class “A”, “B” or “C” — 18 years of age
 Interstate (across state lines) Class “A”, “B” or “C” — 21 years of age

RESTRICTED COMMERCIAL DRIVER’S LICENSE

The Restricted Commercial Driver’s License (RCDL), when accompanied by a Seasonal Permit (SEP), allows Nebraska residents 18 years of age or older to drive Class “B” or “C” Commercial Motor Vehicle (CMV) for purposes of farm-related or ranch-related service industries within 150 miles of the employer’s place of business or the farm or ranch currently being served.

RCDL holders may only transport hazardous materials requiring placards when transporting:

1. Diesel fuel in quantities of 1,000 gallons or less;
2. Liquid fertilizers in vehicles or implements of husbandry with total capacities of 3,000 gallons or less; or
3. Solid fertilizers not transported or mixed with any organic substance.

Applicants must hold a valid operator’s license for at least one year prior to application. No knowledge or skills tests are required. Two separate license documents will be issued: A RCDL and a SEP. Individuals may operate commercial motor vehicles when both documents are valid. Individuals may operate a car when the RCDL is valid without the SEP. The SEP is good for 180 consecutive days in any 12-month period.

ENDORSEMENTS

CODE	ENDORSEMENT
H	Hazardous Material
N	Tank Vehicle
M	Motorcycle
P	Passenger
S	School Bus
T	Double/Triple Trailers
X	Combination Tank/HM

SCHOOL BUS ENDORSEMENT

School bus drivers who are required to hold a CDL must also have a Passenger (P) and School Bus (S) endorsement. The (P) and (S) endorsements require a written and a skills test.

HAZARDOUS MATERIALS ENDORSEMENT - SECURITY THREAT ASSESSMENT

Prior to the issuance of a Nebraska CDL with the hazardous materials endorsement, individuals will be required as per the USA PATRIOT Act to have a Transportation Security Administration (TSA) security threat assessment completed. The security threat assessment procedures require individuals to complete a hazardous materials application and be fingerprinted—a fee will be charged. This process will be handled by a TSA third party agent—**not** the Nebraska DMV.

To complete the hazardous materials endorsement application and to find fingerprinting locations go to <https://hazprints.tsa.dhs.gov/Public/>. For questions regarding the application contact the TSA toll-free at 1-877-429-7746.

The hazardous materials application must be completed before going to a fingerprint collection location.

TANK ENDORSEMENT

Tank vehicle means any commercial motor vehicle that is designed to transport any liquid or gaseous materials within a tank that is either permanently or temporarily attached to the vehicle or the chassis. Such vehicle includes, but is not limited to, a cargo tank and a portable tank, as defined in 49 CFR 171. However, this definition does not include a portable tank that has a rated capacity under one thousand gallons. A commercial motor vehicle hauling any portable tank with a maximum capacity greater than 119 gallons that is not permanently attached to a vehicle, but is loaded and unloaded without being removed from the vehicle is to be treated the same as if it were a permanently attached cargo tank. In these instances, the exception for portable tanks with capacities less than one thousand gallons would **not** apply and the tank endorsement would be required. A commercial motor vehicle hauling a portable tank having a rated capacity under one thousand gallons does **not** need the tank endorsement, provided the tank is filled before being loaded on the truck and then emptied after being unloaded from the truck.

RESTRICTIONS

CODE	RESTRICTION
B	Corrective Lenses
C	Mechanical Aids on Vehicle
E	Automatic Transmission
F	Outside Mirrors
G	Limited to Daylight Only
I	Impaired - Intrastate Only
K	Intrastate Only
L	Vehicle Without Air Brakes
M	Except Class A Bus

CODE	RESTRICTION
N	Except Class A & B Bus
O	Except Tractor Trailer
Q	No Interstate Driving
U	Automatic Turn Signals Req'd
V	Geographical Area (Specify)
W	No One Way Streets/Roadways
X	Maximum Speed Restriction
Y	2 Lane, 2 Way Traffic Only
Z	Special, as Specified by DMV

TESTING AND RENEWAL

Individuals applying for an original (first time) CDL will be required to pass the knowledge and skills tests. Individuals renewing their Nebraska CDL will not be required to take the knowledge tests, except for the hazardous materials portion, if the renewal is for the same class of commercial motor vehicle. The knowledge and skills tests, except for the hazardous material test, may be waived if the applicant surrenders to Nebraska a valid CDL from another state and is applying for the same class of license. Skills tests are given on an appointment only basis.

A knowledge test is required for:

- the class of vehicle;
- each endorsement;
- removing the air brake restriction; and
- pre-trip vehicle inspection.

Any person applying for or renewing any CDL must provide the DMV a copy of their current medical certificate. Anyone possessing a CDL renewable after January 30, 2014 must provide a copy of their current medical certificate to the DMV no later than January 30, 2014. Failing to file a medical certificate with the DMV when required will result in **cancellation of the entire license**, not just the CDL portion.

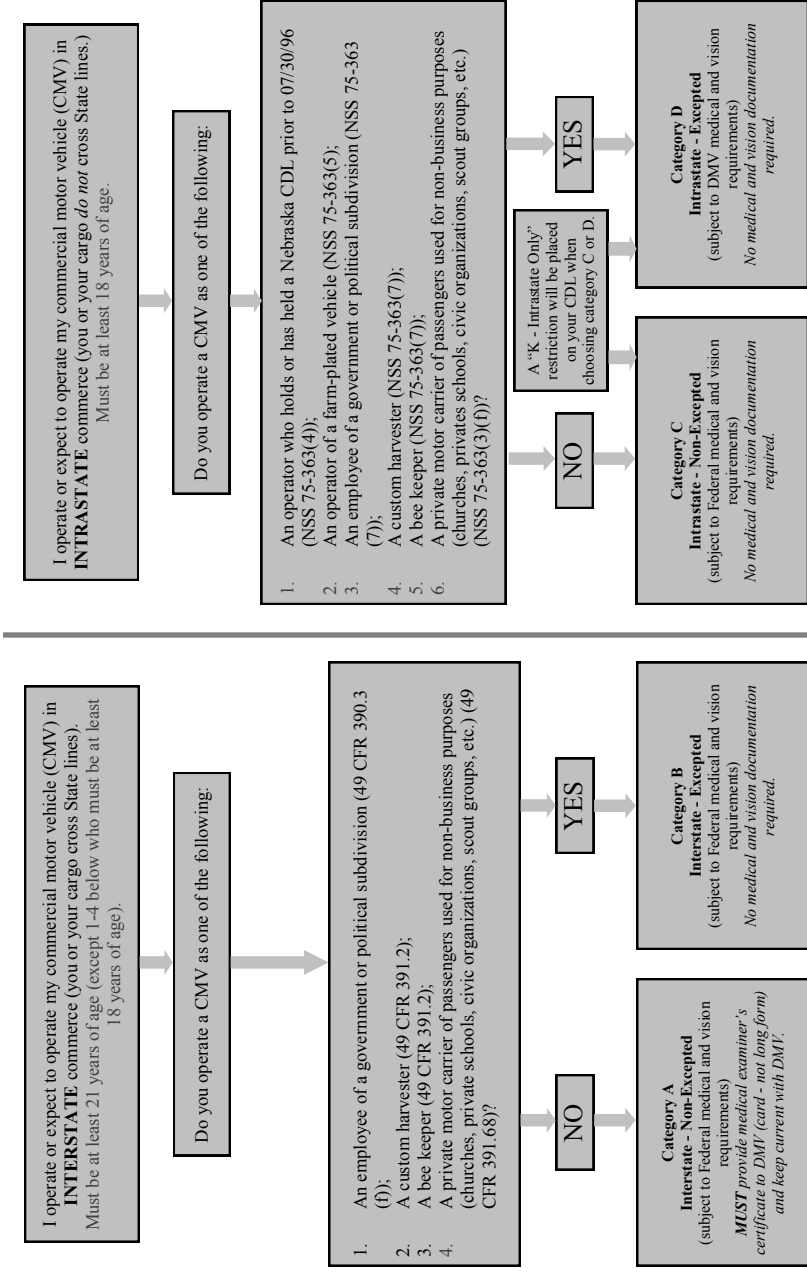
For further information contact:

The Department of Motor Vehicles at 402-471-3861 or www.dmv.ne.gov.

The CDL manual is available online at www.dmv.ne.gov/examining/pdf/cdlmanual.pdf.

Other licensing manuals are available online at www.dmv.ne.gov/examining/.

CDL SELF CERTIFICATION CATEGORIES



Please access the DMV website at www.dmv.ne.gov for additional information regarding the new CDL requirements and this chart. Pay particular attention to the Frequently Asked Questions on the DMV website.

CDL DISQUALIFICATIONS

Disqualification is the loss of your CDL that will occur if you lose any driving privilege in this or any other state through suspension, revocation, cancellation, administrative determination or upon conviction of certain offenses. *Refer to following sections.*

MAJOR OFFENSES

Any CDL holder convicted of using ANY motor vehicle in the commission of a felony involving the manufacture, distributing or dispensing of a controlled substance will be disqualified for life.

Operating Any Motor Vehicle (Not Limited to Operation of a CMV)	Operating a CMV (Which Requires a CDL)
Refusing to submit to an alcohol or drug test while operating ANY motor vehicle.	Driving a CMV while the driver's blood alcohol level is 0.04% or more.
Driving ANY motor vehicle under the influence of alcohol or a controlled substance.	Driving a CMV after the driver's CDL has been suspended, revoked, cancelled or the driver is disqualified from driving a CMV.
Leaving the scene of an accident driving ANY motor vehicle.	Causing a fatality through the negligent or criminal operation of a CMV.
Using ANY motor vehicle in the commission of a felony.	

Major Traffic Offenses	Disqualification	Disqualification Operating CMV Requiring HM Placards
1st conviction	1 year	3 years
2nd conviction	Lifetime	Lifetime

SERIOUS TRAFFIC OFFENSES

When applicable, if any of the below offenses occur while the CDL holder is operating a *non-CMV* and the *court conviction results in the license being revoked, cancelled, suspended or impounded by the court*, it will be considered a serious offense and count towards disqualification.

Operating ANY CMV	
Speeding at or in excess of 15 miles per hour over the posted speed limit.	Driving a CMV without a current CDL in driver's possession.
Making improper or erratic lane changes.	Driving a CMV without obtaining a CDL.
Any violation of any law or ordinance related to motor vehicle traffic control arising in connection with an accident or collision resulting in a fatality.	Driving a CMV without the proper class or endorsement for the specific vehicle being operated.
Willful reckless or reckless driving.	Following the vehicle ahead too closely.

Serious Traffic Offenses Within 3 Years	Disqualification
2nd conviction	60 days
3rd or subsequent conviction	120 days

OUT-OF-SERVICE ORDERS

Disqualification following conviction during any 10-year period for violating an out-of-service order while operating a CMV:

Out-of-Service Violation	Disqualification	Disqualification Operating CMV Requiring HM Placards
1st conviction	180 days, not more than 1 year	180 days, not more than 2 years
2nd conviction	2 years, not more than 5 years	3 years, not more than 5 years
3rd or subsequent conviction	3 years, not more than 5 years	3 years, not more than 5 years

RAILROAD GRADE CROSSING VIOLATIONS

Conviction during any 3-year period for operating a CMV in violation of the following:

For Drivers <i>Not Always</i> Required to Stop	For Drivers <i>Always</i> Required to Stop	For <i>All</i> Drivers
Failing to slow down and check that tracks are clear.	Failing to stop before driving into the crossing.	Failing to have sufficient space to drive completely through the crossing without stopping.
Failing to stop before reaching crossing if tracks are not clear.		Failing to obey a traffic control device or the directions of an enforcement official at the crossing.
		Failing to negotiate a crossing because of insufficient undercarriage clearance.

Railroad Grade Crossing Violation	Disqualification
1st conviction	Not less than 60 days
2nd conviction within 3 years	Not less than 120 days
3rd or subsequent conviction within 3 years	Not less than 1 year.

The following vehicles are *always* required to stop at railroad grade crossings:

- Buses transporting passengers for-hire or *ANY* school bus; exceptions include:
 - A peace officer or flag-person directing traffic to proceed; or
 - An abandoned or exempt crossing when it is clearly marked.
- Vehicles required to display hazardous materials placards; exceptions include:
 - A peace officer or flag-person directing traffic to proceed; or
 - An abandoned or exempt crossing when it is clearly marked.
- Heavy Equipment, including crawler-type tractor, steam shovel, derrick, roller, or any equipment or structure having a normal operating speed 10 mph or less or a vertical body or load clearance of less than 1/2" per foot of the distance between any 2 adjacent axles or in any event of less than 9", measured above the level surface of a roadway.
 - If a flag-person is provided by the railroad, movement over the crossing shall be under his or her direction.

WEIGH STATIONS

Which vehicles need to stop at weigh stations?

All pickup trucks with a factory-rated capacity of more than one ton, pickup trucks pulling trailers, truck-tractors, and trucks must stop at weigh stations. This includes rental trucks such as Ryder, Penske, or U-Haul, whether or not they are involved in commerce. Recreational vehicles are exempt.

I have PrePass and am driving toward an open weigh station in Nebraska. Do I follow my in-cab signal or do I obey the roadside signs that tell me to exit or bypass?

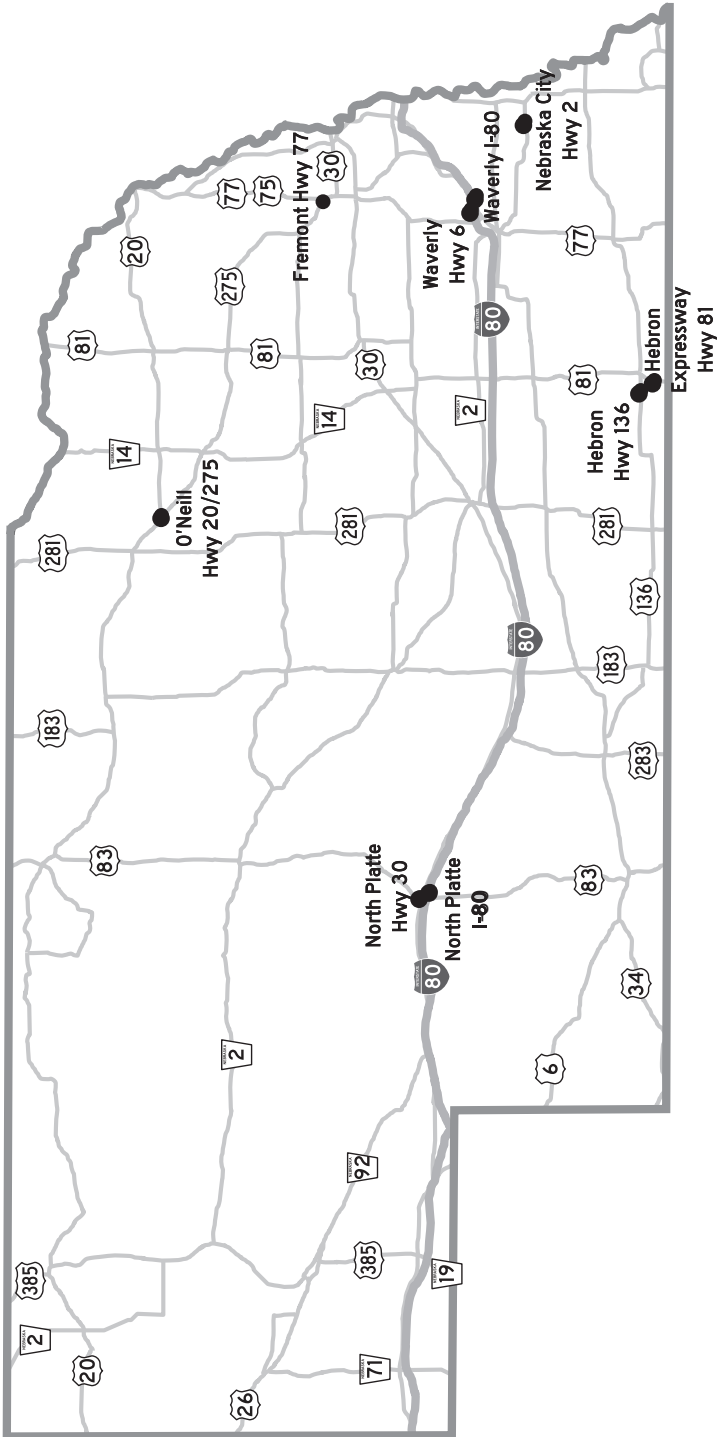
In Nebraska, unless you are operating under an overdimensional or overweight permit, you should always follow your in-cab signal and ignore the roadside signs. When operating under an overdimensional or overweight permit you must always exit to an open weigh station, no matter what the in-cab signal or roadside signs instruct you to do.

Contact Information for Weigh Stations

Scale	Phone	Highway
Fremont	402-654-2801	Highway 77
Hebron Expressway	402-324-5106	Highway 81
Hebron	402-324-5106	Highway 136
Nebraska City	402-873-1549	Highway 2
North Platte I-80 Eastbound	308-535-6612	Interstate 80
North Platte I-80 Westbound	308-535-6606	Interstate 80
North Platte Hwy 30	308-535-8040	Highway 30
O'Neill	402-394-5445	Highway 20/275
Waverly I-80 Eastbound	402-786-7084	Interstate 80
Waverly I-80 Westbound	402-786-7091	Interstate 80
Waverly Hwy 6	402-786-2681	Highway 6



MAP OF WEIGH STATIONS



SIZE AND WEIGHT

WEIGHTS

Maximum allowable weights:

Any single axle – 20,000 lbs.

Any tandem axle – 34,000 lbs.

Maximum weight on State highways – 95,000 lbs.

(refer to bridge chart on page 16)

Maximum Weight on Interstate – 80,000 lbs.

95,000 lbs. with Conditional Interstate Use Permit

(refer to bridge chart on page 16)

Tandem Axle: any two consecutive axles whose centers are more than 40” and not more than 96” apart, measured to the nearest inch between any two adjacent axles in the series.

The maximum gross load on any group of two axles, the distance between the extremes of which is more than 8’ but less than 8’6”, shall be 38,000 pounds.

The distance between axles shall be measured to the nearest foot. When a fraction is exactly one-half foot, the next larger whole number shall be used, except that any group of 3 axles shall be restricted to a maximum load of 34,000 pounds unless the distance between the extremes of the first and third axle is at least 96” in fact.

In all cases gross weights are subject to all wheel and axle load restrictions indicated in the table on the following page, *except* if you have two consecutive sets of tandem axles that measure a minimum of 36’, 37’, or 38’, you may carry 34,000 pounds each on such consecutive sets of tandem axles.

Sliding Fifth-Wheel: It shall be unlawful to reposition the fifth-wheel connection device of a truck-tractor and semitrailer combination which is carrying cargo and on the state highway system, except for fifth-wheel repositioning done pursuant to Section 60-6,301 of the State Statutes.

Weight Tolerance: None. There is a 5% weight shift if overweight on only one axle, one tandem axle, or one group of axles when the distance between the first and last axle of such group of axles is 12’ or less.

Dummy Axles shall be disregarded in determining the legal weight of a vehicle or combination of vehicles if the dummy axle does not carry the lesser of 8,000 lbs. or 8% of the gross weight of the vehicle, or vehicle combination *including* the load.

Idle Reduction Technology or APU: The maximum gross weight limit and the axle weight limit for any vehicle or combination of vehicles equipped with an APU may be increased by an amount necessary to cover the additional weight of the APU. The additional weight shall not exceed 400 lbs. and shall not be in addition to the 5% shift tolerance.

Distance in feet
between the
extremes of
any group of
two or more

BRIDGE CHART

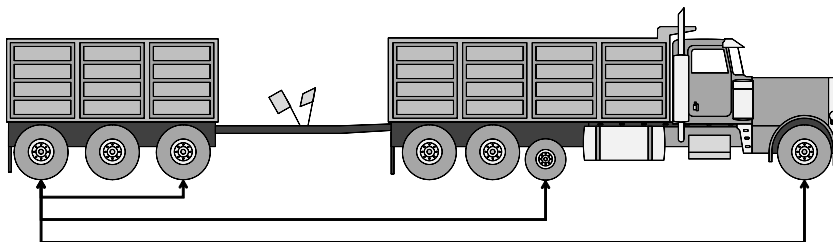
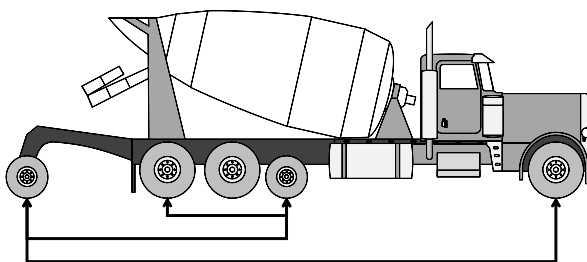
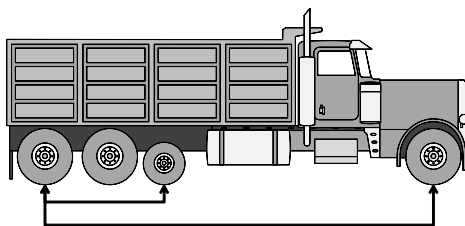
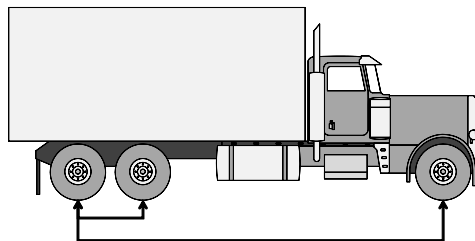
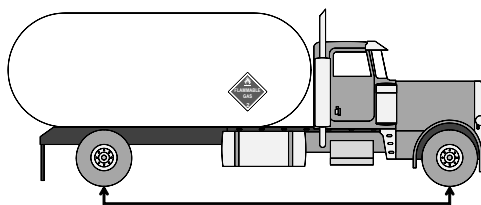
Maximum load in pounds carried on any group

<u>consecutive axles</u>	<u>2 Axles</u>	<u>3 Axles</u>	<u>4 Axles</u>	<u>5 Axles</u>	<u>6 Axles</u>	<u>7 Axles</u>
4	34,000					
5	34,000					
6	34,000					
7	34,000					
8	34,000	42,000				
9	39,000	42,500				
10	40,000	43,500				
11		44,000				
12		45,000	50,000			
13		45,500	50,500			
14		46,500	51,500			
15		47,000	52,000			
16		48,000	52,500	58,000		
17		48,500	53,500	58,500		
18		49,500	54,000	59,000		
19		50,000	54,500	60,000		
20		51,000	55,500	60,500		
21		51,500	56,000	61,000		
22		52,500	56,500	61,500		
23		53,000	57,500	62,500		
24		54,000	58,000	63,000		
25		54,500	58,500	63,500	69,000	
26		55,500	59,500	64,000	69,500	
27		56,000	60,000	65,000	70,000	
28		57,000	60,500	65,500	71,000	
29		57,500	61,500	66,000	71,500	
30		58,500	62,000	66,500	72,000	
31		59,000	62,500	67,500	72,500	
32		60,000	63,500	68,000	73,000	
33			64,000	68,500	74,000	
34			64,500	69,000	74,500	
35			65,500	70,000	75,000	
36			66,000	70,500	75,500	
37			66,500	71,000	76,000	81,500
38			67,500	72,000	77,000	82,000
39			68,000	72,500	77,500	82,500
40			68,500	73,000	78,000	83,500
41			69,500	73,500	78,500	84,000
42			70,000	74,000	79,000	84,500
43			70,500	75,000	80,000	85,000
44			71,500	75,500	80,500	85,500
45			72,000	76,000	81,000	86,000
46			72,500	76,500	81,500	87,000
47			73,500	77,500	82,000	87,500
48			74,000	78,000	83,000	88,000
49			74,500	78,500	83,500	88,500
50			75,500	79,000	84,000	89,000
51			76,000	80,000	84,500	89,500
52			76,500	80,500	85,500	90,500
53			77,500	81,000	86,000	91,000
54			78,000	81,500	86,500	91,500
55			78,500	82,500	87,000	92,000
56			79,500	83,000	87,500	92,500
57			80,000	83,500	88,000	93,000
58				84,000	89,000	94,000
59				85,000	89,500	94,500
60				85,500	90,000	95,000

Correct Method for Measuring Groups of Axles

Refer to bridge chart on page 16

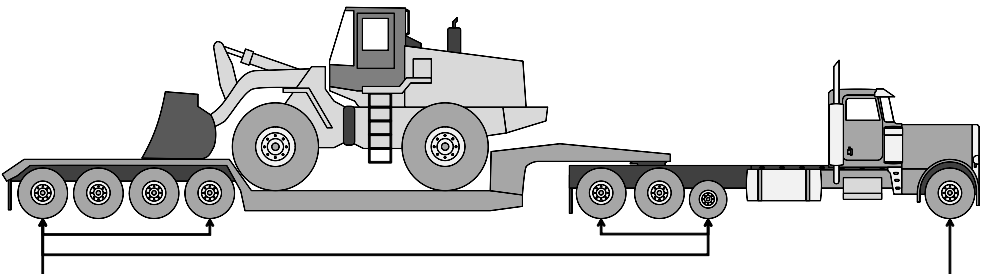
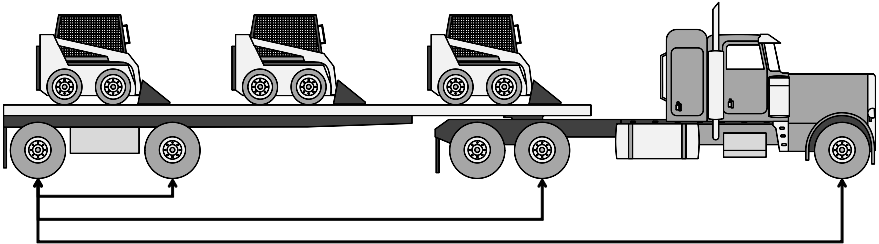
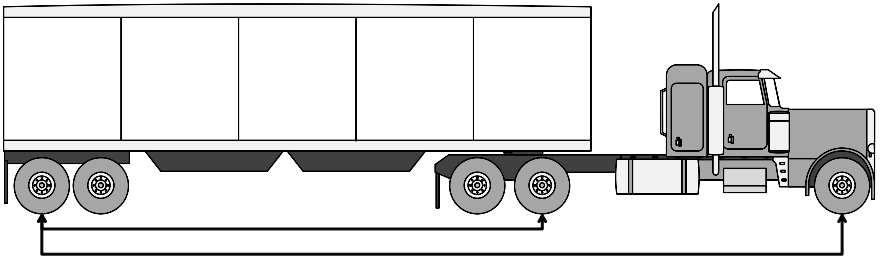
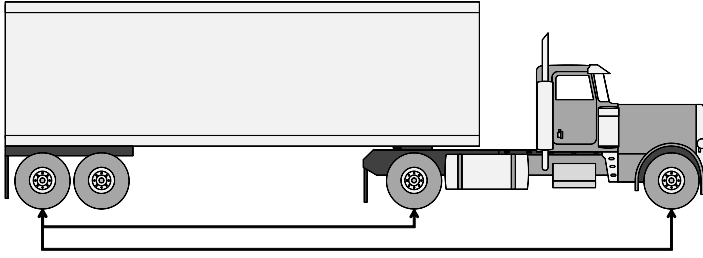
Not all measurements shown on all combinations



Correct Method for Measuring Groups of Axles

Refer to bridge chart page 16

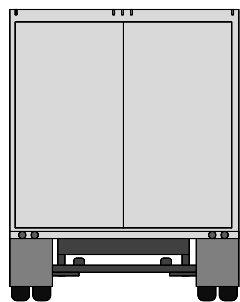
Not all measurements shown on all combinations



Dimensions

All dimension measurements include load.

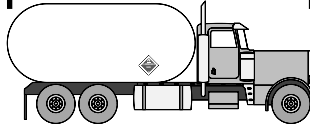
Maximum Width 8' 6"



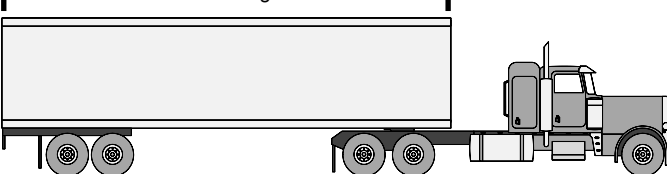
Maximum Height 14' 6"

Width 8' 6" (including load)
 Height 14' 6" (including load)
 Length - single vehicle 40' (including load)
 Exceptions may apply

Maximum Length 40'

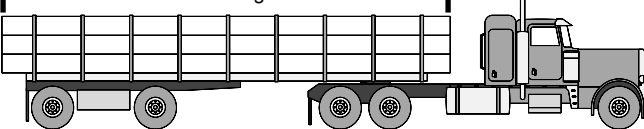


Maximum Length 53'



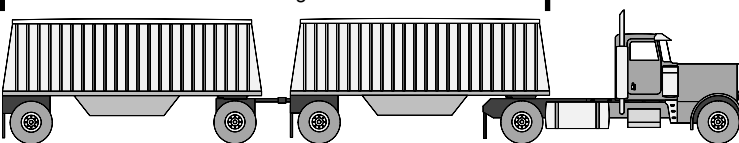
Maximum overall length semitrailer 53' (including load, excluding truck-tractor)

Maximum Length 53'



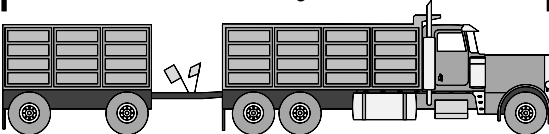
Maximum overall length semitrailer 53' (including load, excluding truck-tractor)

Maximum Length 65'



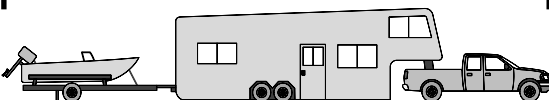
Maximum overall length semitrailer and trailer 65' (including load and connecting devices, excluding truck-tractor)

Maximum Length 65'



Maximum overall length for any other combination 65' (including load)

Maximum Length 65'



Maximum overall length for any other combination 65' (including load)

Width - Maximum Legal Width on All Highways – 8'6"

Exceptions for width only (*exceptions do not apply to Interstate Highway System*):

1. Farm equipment in temporary movement during daylight hours, or hours of darkness when the clearance light requirements are complied with in the normal course of farm operations.
2. Combines 18' or less in width, while in the normal course of farm operations and while being driven during daylight hours or during hours of darkness when the clearance light requirements are fully complied with.
3. Combines in excess of 18' in width, while in the normal course of farm operations and while being driven during daylight hours for distance of 25 miles or less on highways and while preceded by a well-lighted pilot vehicle or flag person, and during hours of darkness when clearance light requirements are met.
4. Combines and vehicles used in transporting combines or other implements of husbandry, and only when transporting combines or other implements of husbandry, to be engaged in harvesting or other agricultural work, while being transported into or through the state during daylight hours, when the total width including the width of the combine or other implement of husbandry being transported does not exceed 15', except that vehicles used in transporting combines or other implements of husbandry can, when necessary to the harvesting operation or other agricultural work, travel unloaded for distances not to exceed 25 miles, while the combine or other implement of husbandry to be transported is engaged in a harvesting operation or other agricultural work.
5. Livestock forage vehicles loaded or unloaded that comply with livestock forage vehicle provisions of 60-6,305 (*not exceeding 65' length, 18' width, 18' height, and being operated only during hours of daylight*).
6. During daylight hours only, vehicles en route to pickup, delivering or returning unloaded from delivery of baled livestock forage which, including the load if any, may be 12' in width.
7. Farm equipment dealers hauling, driving, delivering or picking up farm equipment or other implements of husbandry during daylight hours.

Height – Maximum Legal Height on All Highways – 14'6"

Exceptions for height only (*exceptions do not apply to Interstate Highway System*):

1. Combines or vehicles used in transporting combines, to be engaged in harvesting within or out of the state, moving into or through the state during daylight hours when overall height does not exceed 15'6".
2. Farm equipment dealers hauling farm equipment shall not exceed overall height of 15'6".
3. Livestock forage vehicles with or without load that comply with subsection 2 of 60-6,305. (*Refer to #5 above for dimensions*)
4. Vehicles hauling baled livestock forage shall not exceed overall height of 15'6".

Owners, lessees and operators of a vehicle exceeding 12'6" in height shall assume the risk of loss to the vehicle or its load and shall be liable for any damages that result to overhead obstructions.

Length – Maximum Legal length on All Highways

Single Vehicles shall not exceed 40' length

Exceptions:

1. A truck-tractor.
2. A semitrailer operated in a truck-tractor single semitrailer combination.

Truck and Trailer combinations shall not exceed a overall length of 65', including front and rear bumpers and load.

Exceptions:

1. One truck and one trailer loaded or unloaded, used in transporting implements of husbandry to be engaged in harvesting, while being transported into or through the state during daylight hours if the total length does not exceed 75' including the load.
2. A truck-tractor single semitrailer combination.
3. A truck-tractor semitrailer-trailer combination, but the semitrailer-trailer portion shall not exceed 65' including connecting devices.
4. A driveway saddlemount vehicle transporter combination and driveway saddlemount with fullmount vehicle transporter combination, but the total overall

- length shall not exceed 97'.
5. The movement of public utility or other construction and maintenance material at any time.

The following over length vehicles are not authorized to travel on the Interstate Highway System:

1. Movement of unbaled livestock forage vehicles, loaded or unloaded.
2. The overhang of a combine to be engaged in harvesting, while being transported into or through the state driven during daylight hours by a truck-tractor semitrailer combination, but the length of the semitrailer including overhang shall not exceed 63' and the maximum semitrailer length shall not exceed 53'.
3. Farm equipment dealers hauling, driving, delivering or picking up farm equipment within the county in which the dealer maintains his place of business, or adjoining counties and return.
4. Temporary movement of farm machinery during daylight hours in normal farm operation.

Custom Harvest Information

Not authorized on the Interstate Highway System

Maximum Legal Width - 8'6"

Exception:

- Combines and vehicles used in transporting combines or other implements of husbandry, and only when transporting combines or other implements of husbandry, to be engaged in harvesting or other agricultural work, while being transported into or through the state during daylight hours, when the total width including the width of the combine or other implement of husbandry being transported does not exceed 15', except that vehicles used in transporting combines or other implements of husbandry may, when necessary to the harvesting operation or other agricultural work, travel unloaded for distances not to exceed 25 miles, while the combine or other implement of husbandry to be transported is engaged in a harvesting operation or other agricultural work.
- Mobile homes not exceeding 16' in width and with an outside tire width dimension not exceeding 120" and a length not exceeding 40' moving during daylight hours.

Maximum Legal Length of a Straight Truck-Trailer Combination - 65'

Overall Exceptions:

- One truck and one trailer, loaded or unloaded, used in transporting implements of husbandry to be engaged in harvesting, while being transported into or through the state during daylight hours if the total length does not exceed 75' including load.

Maximum legal length of Truck-Tractor Combinations

- Semitrailer is 53' including load.
- Truck-tractor semitrailer, trailer combination is 65' measuring the trailers only including the load.
- All other combinations (including B-Trains) 65' including load.
- With a truck-tractor with two trailers or a truck-tractor with a semitrailer and trailer combination used to transport custom harvesting equipment for agricultural producers to harvest wheat, soybeans, or milo from April through November, the maximum length of the trailers is 81' 6", including any coupling device or devices, but not including the power unit.

Maximum Legal Height for a Vehicle Empty or Loaded - 14'6"

Exception:

- Combines or vehicles used in transporting combines, to be engaged in harvesting within or outside the state, moving into or through the state during daylight hours when the overall height does not exceed 15'6".
- Vehicles which have been issued an overdimensional permit.

Note: Owners, lessees and operators of a vehicle exceeding 12'6" in height shall assume the risk of loss to the vehicle or its load and shall be liable for any damages that result to overhead obstructions.

Overdimensional-Overweight Permits

*A profile will be required before purchasing permits online and an application can be obtained online or by calling the Lincoln Permit Office
Mon.—Fri. during regular business hours.*

Permits may be obtained from the Department of Roads website:
www.truckpermits.nebraska.gov

Lincoln Permit Office
Nebraska Dept. of Roads
Permit Office - Room 123A
500 S 14th St.
Lincoln, NE 68512
402-471-0034
Fax: 402-479-3906

District 2 - Omaha
4425 South 108th St
Omaha, NE 68145
402-595-2534

District 3 - Norfolk
408 North 13th St.
Norfolk, NE 68702
402-370-3470

District 4 - Grand Island
211 North Tilden St.
Grand Island, NE 68802
308-385-6265

District 5 - Gering
140375 Rundell Rd.
Gering, NE 69341
308-436-6587

District 6 - North Platte
1321 North Jeffers
North Platte, NE 69103
308-535-8031

District 7 - McCook
619 Auditorium Dr.
McCook, NE 69001
308-345-8490

District 8 - Ainsworth
736 E 4th St.
Ainsworth, NE 69210
402-387-2471

Nebraska Department of Roads Permits

Single-trip	Overdimensional Only Overdimensional and Overweight Two Axle Flotation	Overweight Only Self-Propelled Equipment
Manufactured Home	New/Dealer Pre-Owned	
Continuous	Statewide Envelope Vehicle Local Overdimensional Local Overdimensional/Overweight Interstate Self Propelled	Statewide Empty Semitrailer Local Overweight Local Self-Propelled Flotation
Other	Conditional Interstate Use Garbage/Refuse Annual Implement of Husbandry for I-80 Only – <i>Dealers Only</i>	Building/Slow Moving Large Object Seasonally Harvested Products

Lincoln & Omaha Travel Restrictions for Permitted Loads

Omaha - State Highways & Interstates: 7am - 9am & 4pm - 6pm (Monday - Friday)
Travel prohibited after 12pm on Saturday & Sunday

Lincoln - State Highways: 7am - 9am & 4pm - 6pm (Monday - Friday)
Travel prohibited after 12pm on Saturday & Sunday

Nebraska Cornhusker Home Football Games: Refer to your permit.

For road conditions during inclement weather call 511.

Seasonal Harvest Overweight Permits

Seasonal harvest permits may be issued to carriers by the Nebraska Department of Roads Permit Offices for hauling grain or seasonally harvested products from the field where they are harvested to storage, market or stockpile in the field for a distance up to 120 miles. These vehicles may be up to 15% greater than the maximum weight specified by law and up to 10% greater than the maximum length specified by law. The distance limitation may be waived for vehicles when carrying dry beans from the field where harvested to storage or market when dry beans are not normally stored, purchased, or used within the permittee's local area and must be transported more than 120 miles to an available marketing or storage destination.

Permits for grain or other seasonable harvested products shall be valid for 30 days or 60 days and renewable for a total number of days not to exceed 150 days per year. No permit shall authorize a weight greater than 20,000 lbs. on any single axle.

In order for the permit to be valid, vehicle(s) must be registered for the maximum gross weight, according to 60-6,294 (see bridge chart page 16).

Overweight Exceptions for Seasonally Harvested Products

No permit is required for carriers hauling grain or other seasonally harvested products operating from the field where such grain or products are harvested to storage, market, or stockpile in the field or from stockpile to market or factory up to 70 miles with a load that exceeds the maximum load permitted by section 60-6,294 by 15% on any tandem axle, group of axles and gross weight. A truck with no more than one single rear axle may also exceed the axle weight and gross weight by 15%. The owner or a representative of the owner of the agricultural product shall furnish the driver of the loaded vehicle a signed statement of origin and destination.

The provisions in these paragraphs do not apply to the National System of Interstate and Defense Highways.

Non-Resident Grain Haulers Permit

A truck, truck-tractor, semitrailer or trailer lawfully licensed in another state or province, not apportion licensed, and engaged in hauling grain or other seasonally harvested products from the field where they are harvested to storage or market must obtain a non-resident grain haulers permit. This permit is valid for 90 days from the time of purchase between June 1 and December 15 and may be purchased from the County Treasurer of the county in which the grain first is hauled. The cost of the permit is \$20 for a straight truck and \$150 for combination units (truck and trailer or truck-tractor semitrailer, or any type combination).

REGISTRATION AND LICENSING

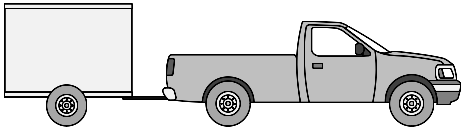
NEBRASKA VEHICLE REGISTRATION

If you are a Nebraska resident operating your vehicles in intrastate (Nebraska-only) commerce, license them at your county courthouse.

If the truck is licensed as a Nebraska apportioned vehicle, the registration expires December 31 each year, with a grace period until January 31. Enforcement begins February 1.

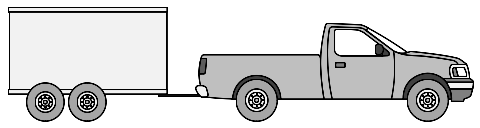
If the truck is licensed in Nebraska at the county level, the registration expires on the first day of the month, one year from the month of issuance. You must renew it before the first day of the following month. Truck and trailer combinations that are registered for weight separately are not allowed to combine registered weight.

Nebraska Commercial-Plated Vehicles



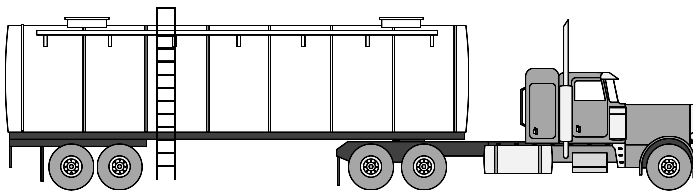
A commercial truck pulling a utility trailer:

- Utility trailer must be licensed separately from the truck.
- Maximum licensed weight for a utility trailer is 9,000 lbs.
- When the trailer weighs more than 9,000 lbs., inclusive of load, the utility trailer must be licensed as commercial trailer.

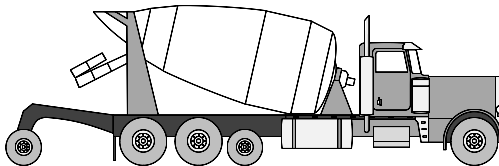


A commercial truck pulling a commercial trailer:

- Power unit must be registered for empty weight of both vehicles plus the maximum load to be carried thereon at any one time.
- Example – 20,000 lbs. (10 tons)

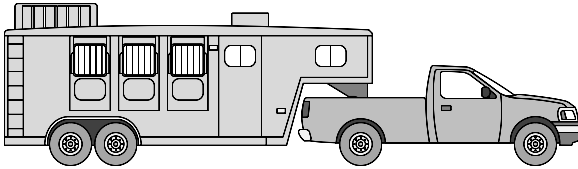


The power unit in a combination vehicle must be registered for empty weight of both vehicles plus the maximum load to be carried thereon at any one time. Example – 80,000 lbs. (40 tons)



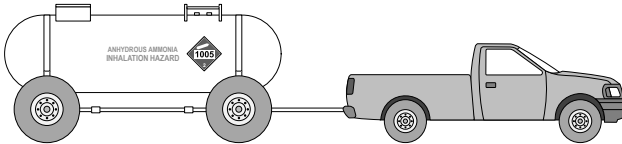
A single vehicle must be registered for empty weight plus the maximum load to be carried thereon at any one time. Example – 54,000 lbs. (27 tons)

Nebraska Farm-Plated Vehicles



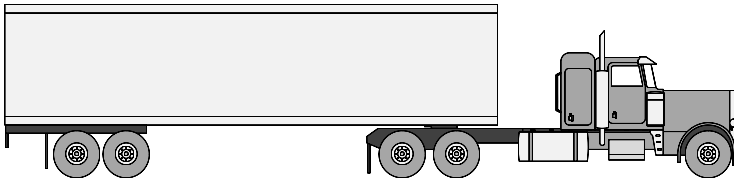
Farm-plated truck and trailer combinations must be licensed one of two ways:

1. Power unit is registered for combined weight of truck, trailer and load, for example, 32,000 lbs. (16 ton); or
2. Power unit and trailer are registered separately for individual weight, inclusive of load, for example, 10,000 lbs. (5 ton) truck plate, 20,000 lbs. (10 ton) trailer plate.



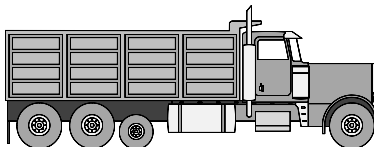
Farm-plated truck pulling a fertilizer trailer.

- Farm truck must be registered for its own weight; and
- Fertilizer trailers are only registered at 20,000 lbs., which is the maximum allowed weight.



Farm plated truck-tractor and semitrailer combinations must be licensed one of two ways:

1. Power unit is registered for the combined weight of the truck, trailer and load, for example, 80,000 lbs. (40 ton) farm plate on the power unit with the trailer registered for no licensed weight; or
2. Power unit and trailer are registered separately for individual weight, inclusive of load, for example, 46,000 lbs. (23 ton) farm plate on the power unit with a 34,000 lbs. (17 ton) farm plate on the trailer.



A single vehicle must be registered for the empty weight plus the maximum load to be carried thereon at any one time, for example, 48,000 lbs. (24 ton) farm plate.

NON-RESIDENT VEHICLE REGISTRATION

If you are a nonresident operating your vehicles strictly in intrastate (Nebraska-only) commerce, you must either:

1. License your vehicles at the county level by purchasing a non-resident commercial plate;
2. Purchase a 72-hour prorated permit; or
3. Apportion your vehicle with Nebraska through your home state.

If you are a nonresident and operate your vehicles in interstate (crossing state lines) commerce, or if you are a Nebraska-based interstate operator, contact:

Department of Motor Vehicles
Motor Carrier Services Division
301 Centennial Mall South
PO Box 94729
Lincoln, NE 68509-4729

402-471-4435

INTERNATIONAL REGISTRATION PLAN (IRP)

Alabama	Idaho	Minnesota	New York	Saskatchewan
Alberta	Illinois	Mississippi	North Carolina	South Carolina
Arizona	Indiana	Missouri	North Dakota	South Dakota
Arkansas	Iowa	Montana	Nova Scotia	Tennessee
British Columbia	Kansas	Nebraska	Ohio	Texas
California	Kentucky	Nevada	Oklahoma	Utah
Colorado	Louisiana	New Brunswick	Ontario	Vermont
Connecticut	Maine	Newfoundland	Oregon	Virginia
Delaware	Manitoba	& Labrador	Pennsylvania	Washington
Dist. of Columbia	Maryland	New Hampshire	Prince Edward Is.	West Virginia
Florida	Massachusetts	New Jersey	Quebec	Wisconsin
Georgia	Michigan	New Mexico	Rhode Island	Wyoming

Nebraska is currently a member of the following apportioned registration agreement:

Any carrier engaged in operating a fleet of one or more apportionable vehicles in the state of Nebraska may, in lieu of full county registration, license such a fleet under the apportioned registration laws of the state of Nebraska.

An apportionable vehicle shall mean: any vehicle except recreational vehicles, vehicles displaying restricted plates, city pickup and delivery vehicles, buses used in the transportation of chartered parties, or government-owned vehicles, used for the transportation of persons, for hire, or designed, used or maintained primarily for the transportation of property and is a vehicle qualifying as one of these:

1. A power unit having 2 axles and a gross vehicle weight or registered gross vehicle weight exceeding 26,000 pounds;
2. A power unit having 3 or more axles regardless of weight; or
3. Used in combination, when the gross vehicle weight of such combination exceeds 26,000 pounds.

Prorate Permits

Any carrier based in the previously mentioned jurisdictions having vehicles eligible for apportioned registration but not registered as such, shall be required to meet their registration responsibilities by alternative means, such as:

1. Purchasing a 72-hour prorate permit at the first truck permit station upon entering the state of Nebraska; or
2. Fully licensing the vehicle at the county level as a nonresident commercial vehicle.

A 72-hour prorate permit may be purchased through:

1. The first truck permit station (see page 30) upon entering the state; or
2. A wire service (Comdata, Trans Mid-America).

A 72-hour prorate permit costs \$25.00 with the station charging a 10% issuing fee.

Operating Authority

Intrastate Carriers (within Nebraska)

If you are an Intrastate Carrier operating as a household mover or transporting persons for hire, contact:

Nebraska Public Service Commission
1200 N St. Suite 300
Lincoln, NE 68508
402-471-3101
www.psc.state.ne.us

Interstate Carriers (crossing state lines)

If you are an Interstate Carrier operating for hire and transporting passengers or a regulated or non-exempt commodity, such as manufactured and processed goods, you must contact FMCSA to obtain proper authority.

The Federal Motor Carrier Safety Administration (FMCSA)
1200 New Jersey Ave SE
Washington, DC 20590
800-832-5660
www.fmcsa.dot.gov

A list of non-exempt commodities is available at FMCSA's website www.fmcsa.dot.gov under *Rules and Regulations* in 49 CFR Part 372.115.

Unified Carrier Registration

The Unified Carrier Registration (UCR) replaced the Single State Registration System (SSRS) in 2007. The UCR affects both for-hire and private motor carriers involved in interstate commerce plus freight forwarders, brokers, and leasing companies.

A motor carrier must register with UCR if they operate a commercial motor vehicle in interstate commerce or if they have an interstate DOT number. For the purposes of UCR a commercial motor vehicle is defined as any vehicle or combination of vehicles that:

- Has a gross vehicle weight, gross combination weight, GVWR or GCWR (whichever is greater) of more than 10,000 lbs.;
- Is designed to transport more than 10 passengers including the driver; or
- Is transporting hazardous materials in a quantity requiring placarding.

Other business entities also required to register with UCR are:

- A freight forwarder;
- A freight broker; or
- A commercial vehicle leasing company.

WRITTEN APPLICATION

The Unified Carrier Registration (UCR) application form, instruction sheet, and application information are available at the Nebraska Department of Motor Vehicles, Motor Carrier Services Division website at www.dmv.ne.gov/mcs.

Return completed UCR application forms with appropriate payments to:

Nebraska Department of Motor Vehicles
Motor Carrier Services
301 Centennial Mall South
PO Box 94729
Lincoln, NE 68509-4729
Toll Free 888-622-1222

ONLINE APPLICATION

In lieu of filing a written UCR application with the Nebraska Department of Motor Vehicles, applicants may complete an online UCR application by visiting the national UCR registration website operated by the state of Indiana at www.ucr.in.gov. The registration website is compatible with mobile devices.

No UCR credential will be issued. There is no requirement to carry or display the receipt of UCR payment in the commercial vehicle.

FUEL AND FUEL PERMIT REQUIREMENTS

International Fuel Tax Agreement Guide

Who needs an International Fuel Tax Agreement Permit?

A qualified motor vehicle that:

1. Has two axles and a gross vehicle weight or registered gross vehicle weight exceeding 26,000 pounds;
2. Has three or more axles regardless of weight; or
3. Is used in combination when the gross vehicle weight or registered gross weight of such combination exceeds 26,000 pounds.

Recreational vehicles such as: motor homes, pickup trucks with attached campers, and buses are not qualified motor vehicles when they are used exclusively for personal pleasure by an individual. In order to qualify as a recreational vehicle, the vehicle shall not be used in connection with any business endeavor.

A qualified farm or commercial motor vehicle listed in the above categories 1, 2, or 3 is required to have in their possession an International Fuel Tax Agreement Permit or purchase a 72-hour fuel permit.

A 72-hour fuel permit may be purchased through:

1. The first truck permit station (see page 30) upon entering the state; or
2. A wire service (see page 30).

Untaxed – Dyed Fuel

Fuel purchased for off-road use is non-taxed and dyed to indicate its non-tax status. Dyed fuel is regulated by the Internal Revenue Service (IRS) and generally cannot be used in any vehicle requiring registration. Vehicles designed and used primarily off-road that are operated on the highways incidental to their primary off-road use may be exempt. The presence of dyed fuel in a highway use vehicle can incur substantial fines and penalties.

If you have questions or to report suspected misuse of dyed fuel please contact the Nebraska Department of Revenue Motor Fuels at 1-800-554-FUEL (3835)

PRORATE AND FUEL PERMIT STATIONS

List of Permit Station Locations See map page 32

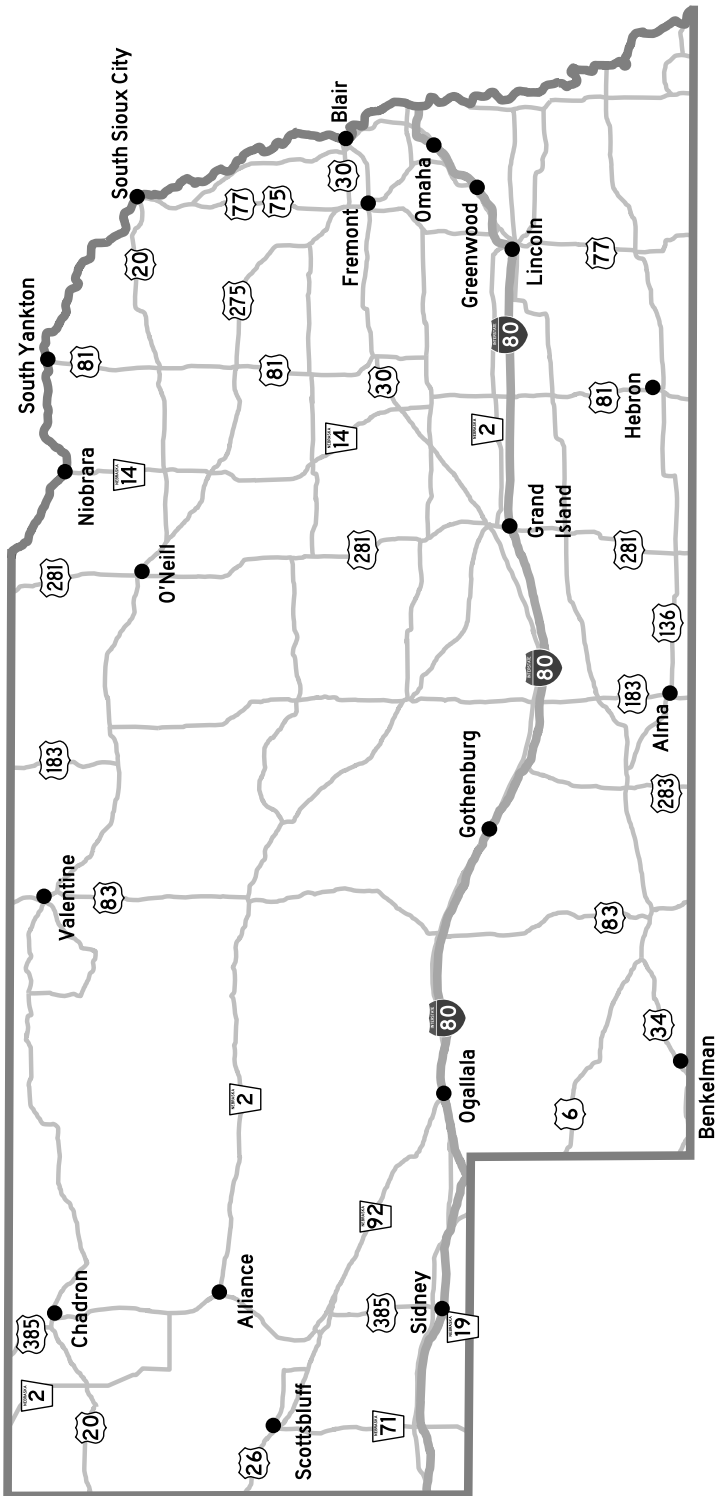
Location	Name/Phone #	Hours
Wire Service	Trans/Mid-America, Inc. (800) 228-7577	24 hours/day 7 days/week
Wire Service	Comdata (800) 749-6058 (800) 749-7166	24 hours/day 7 days/week
Alliance US 385 & NE 2	Western Cooperative Co (308) 762-9902	6AM-11PM Mon-Sat 8AM-9PM Sun
Alma US 136 & US 183	Bosselman Pump & Pantry (308) 928-2590	6AM-9PM 7 days/week
Benkelman NE 61 & US 34	Agland (308) 423-2452	7AM-7PM Mon-Fri 7AM-Noon Sat
Blair Hwy US 30	Petro Mart (402) 426-9150	5:30AM-10PM Mon-Fri 5:30AM-9PM Sat 7AM-9PM Sun
Chadron US 20 & US 385	Big Bat's Shell (308) 432-4504	24 hours/day 7 days/week
Fremont US 77 & US 275	Sapp Bros. (402) 721-7620	24 hours/day 7 days/week
Gothenburg I-80, Exit 211	I-80 Pit Stop (308) 537-3202	24 hours/day 7 days/week
Grand Island US 281/US 34 & I-80, Exit 312	Bosselman Travel Center (308) 382-2288	24 hours/day 7 days/week
Greenwood I-80, Exit 420	Cubby's Greenwood Truck Plaza (402) 944-7055	24 hours/day 7 days/week
Hebron Hwy US 81, MM 13	81 Express (402) 768-2223	6AM-11PM 7 days/week
Lincoln I-80, Exit 395	Shoemakers Truck Station (402) 474-1771	24 hours/day 7 days/week

Location	Name/Phone #	Hours
Lincoln I-80, Exit 395	Don and Randy Shoemaker's Truck Station (402) 438-4800	24 hours/day 7 days/week
Niobrara NE 12 & NE 14	Niobrara Trading Post (402) 857-3460	5AM-10PM 7 days/week
Ogallala NE 61 & I-80, Exit 126	TA Travel Center (308) 284-3667	24 hours/day 7 days/week
Omaha NE 50 & I-80, Exit 440	Sapp Bros. (402) 895-2121	24 hours/day 7 days/week
O'Neill Hwy US 20/US 275	Cubby's (402) 336-3028	6:30AM-9PM 7 days/week
Scottsbluff 1.7 miles S of US26 on Ave I	Western Truck Terminal (308) 635-7374	24 hours/day 7 days/week
Sidney US 385 & I-80, Exit 59	Sapp Bros. (308) 254-3096	24 hours/day 7 days/week
South Sioux City I-129 & US 77	Crystal Oil Co. (402) 494-5471	24 hours/day 7 days/week
Valentine Hwy US 20	Roadrunner Shell (402) 376-2280	6AM-Midnight 7 days/week
S. Yankton Hwy US 81	Yankton Shop - EZ (402) 667-9855	24 hours/day 7 days/week



Nebraska Permit Station Map

See detailed list of permit stations on page 30



GENERAL NEBRASKA REGULATIONS

Vehicles required to stop at weigh stations

All pickup trucks with a factory-rated capacity of more than one ton, pickup trucks pulling trailers, truck-tractors, and trucks must stop at weigh stations. This includes rental trucks such as Ryder, Penske, or U-Haul, whether or not they are involved in commerce. Recreational vehicles are exempt.

Load projecting to rear: red flag, red light required

Whenever the load on any vehicle extends more than 4' beyond the rear of the bed or body thereof, there shall be displayed at the end of such load in such position as to be clearly visible at all times from the rear of such load a red flag not less than 12" both in length and width, except that between sunset and sunrise there shall be displayed at the end of any such load a red light plainly visible under normal atmospheric conditions at least two hundred feet from the rear of such vehicle.

Splash aprons

Every new motor vehicle or semitrailer purchased after January 1, 1956, and operated on any highway in this state shall be equipped with fenders, covers, or devices, including flaps or splash aprons, unless the body of the vehicle affords adequate protection to effectively minimize the spray or splash of water or mud to the rear of the motor vehicle or semitrailer.

Load: requirements

No vehicle shall be driven or moved on any highway unless the vehicle is so constructed or loaded as to prevent its contents from dropping, sifting, leaking, or otherwise escaping from the vehicle.

Transportation of sand, gravel, rock; material covered with canvas

No person shall transport any sand, gravel, rock less than 2" in diameter, or refuse in any motor vehicle on any hard-surfaced state highway if such material protrudes above the sides of that part of the vehicle in which it is being transported unless such material is enclosed or completely covered with canvas or similar covering.

Load securement

No person shall drive or move any motor vehicle, trailer, or semitrailer upon any highway unless the cargo and contents carried by the motor vehicle, trailer, or semitrailer are properly distributed and adequately secured to prevent the falling of cargo or contents from the vehicle. The tailgate, doors, tarpaulins, and any other equipment used in the operation of the motor vehicle, trailer, or semitrailer or in the distributing or securing of the cargo or contents carried by the motor vehicle, trailer, or semitrailer shall be secured to prevent cargo or contents falling from the vehicle. The means of securement to the motor vehicle, trailer, or semitrailer must be either tie downs and tie down assemblies of adequate strength or sides, sideboards, or stakes and a rear end gate, end board, or stakes strong enough and high enough to assure that cargo or contents will not fall from the vehicle.

Truck-trailer combination: warning decal required

A warning decal shall be attached to every truck-trailer combination having a connection device between such vehicles which is more than twelve feet in length. Such decal shall be made of red reflective material and contain the words "Long Vehicle - Pass With Care." The letters shall be of white reflective material and shall be not less than 3" in height. The decal shall be affixed to the sides and rear parts of the trailer at a height of not less than 48" or no more than 74" from the ground level.

Trucks: rear-view mirror

Each truck shall be equipped with a rear-view mirror which shall be kept clean, repaired and installed according to the official highway rules.

Oversized loads: mirrors

Outside mirrors must reflect a view of the highway behind the vehicle to a distance of at least 200’.

Towing: draw bars, other connections; length; red flag required

The draw bar or other connection between any two vehicles, one of which is towing or drawing the other on a highway, shall not exceed 15’ in length from one vehicle to the other, except a vehicle being towed with a connection device that is an integral component of the vehicle and is designed to attach to a lead unit with construction in such a manner as to allow articulation at the attachment point on the chassis of the towed vehicle but not to allow lateral or side-to-side movement. Such connecting device shall meet the safety standards for towbar failure or disconnection that are in effect on March 28, 1980, in the Federal Motor Carrier Safety Regulations of the U.S. Department of Transportation of the United States Government and shall have displayed at approximately the halfway point between the towing vehicle and the towed vehicle on the connecting mechanism a red flag or other signal or cloth not less than 12” both in length and width that shall be at least 5’ and not more than 10’ from the level of the paving and shall be displayed along the outside line on both sides of the towing and towed vehicles. Whenever such connection consists of a chain, rope or cable, there shall be displayed upon such connection a red flag or other signal or cloth not less than 12” both in length and width.

Brakes: requirements

Every motor vehicle when operated upon a highway shall be equipped with brakes adequate to control the movement of and to stop and to hold such vehicle. All such brakes shall be maintained at all times in good working order.

Trailers: brake requirements, safety chains

1. Trailers subject to the Federal Motor Carrier Safety regulations as adopted by Nebraska Statue 75-363 must have brakes as specified in 49 CFR 393.
2. All other commercial trailers with a gross vehicle weight rating of more than 10,000 pounds and semitrailers shall be equipped on each wheel with brakes that can be operated from the driving position of the towing vehicle.
3. Cabin trailers as defined in Sections 60-107 and 60-1,801 and recreational trailers having a gross loaded weight of 3,000 pounds or more but less than 6,500 pounds shall be equipped with brakes on at least two wheels, and such trailers with a gross loaded weight of 6,500 pounds or more shall be equipped with brakes on each wheel. The brakes shall be operable from the driving position of the towing vehicle. Such trailers shall also be equipped with a breakaway, surge, or impulse switch on the trailer so that the trailer brakes are activated if the trailer becomes disengaged from the towing vehicle.
4. Cabin trailers, recreational trailers, and utility trailers as defined in Section 60-301, when being towed upon a highway, shall be securely connected to the towing vehicle by means of two safety chains or safety cables in addition to the hitch or other primary connecting device. Such safety chains or safety cables shall be so attached and shall be of sufficient breaking load strength so as to prevent any portion of such trailer drawbar from touching the roadway if the hitch or other primary connecting device becomes disengaged from the towing vehicle.

REGULATIONS FOR SPECIFIC GROUPS

Other Activities That May Be Subject to Federal Safety Regulations

Activities such as, but not limited to: motor sports (race cars, snowmobiles, motorcycles, watercraft, etc.), fishing tournaments, livestock (cattle, horses) shows, and antique tractor pulls.

1. Do you declare prize money as income from a business expense for tax purposes?
2. Are the costs for the underlying activities deducted as a business expense for tax purposes?
3. Do you accept products and/or money for advertising from a sponsor?
4. Is the transport vehicle being used in the furtherance of a commercial operation?

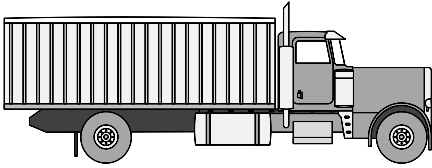
If you answered **YES** to any of the above questions, go to question 5. If you answered **NO** to all the above questions stop here, you are not subject to the Federal Motor Carrier Safety Regulations.

5. Is the GVWR, GCWR, gross vehicle weight, or gross combination weight (whichever is greater) over 10,000 pounds?

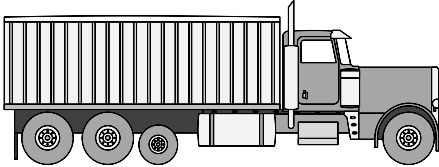
If you answered **YES** to question 5, you must follow the appropriate regulations. If you answered **NO**, you are not subject to the Federal Motor Carrier Safety Regulations.

MOTOR HOMES that are registered as recreational vehicles (RV license plate) **CANNOT** be used in the furtherance of a commercial enterprise. If you wish to use a motor home as part of a commercial enterprise, you must register the motor home as a commercial vehicle.

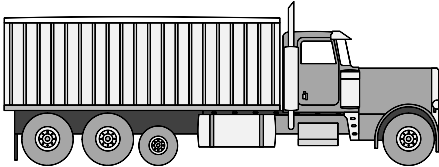
REGULATIONS FOR NEBRASKA FARMERS



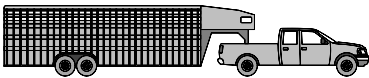
Farm-plated straight truck
Registered for 16 tons or less and
GVWR 10,001 lbs. – 26,000 lbs.



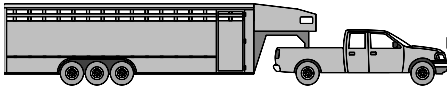
Farm-plated straight truck
Registered for 16 tons or less and
GVWR more than 26,000 lbs.



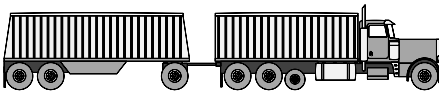
Farm-plated straight truck
Registered for more than 16 tons and
GVWR more than 26,000 lbs.



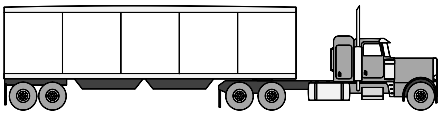
Farm-plated straight truck and trailer
Registered for 16 tons or less and
GVWR combined 10,001 lbs. –
26,000 lbs.



Farm-plated straight truck and trailer
Registered for 16 tons or less and
GVWR combined more than 26,000
lbs.



Farm-plated straight truck and trailer
Registered for more than 16 tons and
GVWR combined more than 26,000
lbs.



Farm-plated truck-tractor, semitrailer
OR farm-plated truck-tractor, semi-
trailer, trailer combination
Registered for more than 16 tons and
GVWR combined more than 26,000
lbs.



Requirements: When Required

(Interstate = crossing state lines. Intrastate = within Nebraska borders)

USDOT Number and Annual Inspections: Interstate travel only

CDL and Drug Testing: Not required

Medical Card and Driver Qualification File: Interstate travel, beyond 150 air miles

Hours of Service: Interstate travel, beyond 100 air-miles of the farm

USDOT Number and Annual Inspections: Interstate travel only

CDL and Drug Testing: Interstate travel or intrastate travel beyond 150 air miles

Medical Card and Driver Qualification File: Interstate travel, beyond 150 air miles

Hours of Service: Interstate travel, beyond 100 air miles of the farm

USDOT Number: Interstate travel only

Annual Inspections: Interstate and intrastate travel

CDL and Drug Testing: Interstate travel or intrastate travel beyond 150 air miles

Medical Card and Driver Qualification File: Interstate travel, beyond 150 air miles

Hours of Service: Interstate travel, beyond 100 air miles of the farm

USDOT Number, Annual Inspections, and Medical Card: Interstate travel only

CDL and Drug Testing: Not required

Driver Qualification File: Interstate travel, beyond 150 air miles

Hours of Service: Interstate, beyond 100 air miles of the farm

USDOT Number, Annual Inspections, and Medical Card: Interstate travel only

CDL and Drug Testing: Interstate travel or intrastate travel beyond 150 air miles

Driver Qualification File: Interstate travel, beyond 150 air miles

Hours of Service: Interstate, beyond 100 air miles of the farm

USDOT Number and Medical Card: Interstate travel only

Annual Inspections: Interstate and intrastate travel

CDL and Drug Testing: Interstate travel or intrastate travel beyond 150 air-miles

Driver Qualification File: Interstate travel, beyond 150 air miles

Hours of Service: Interstate, beyond 100 air miles of the farm

USDOT Number and Medical Card: Interstate travel only

CDL, Drug Testing, and Annual Inspections: Interstate and intrastate travel

Driver Qualification File: Interstate travel, beyond 150 air miles

Hours of Service: Interstate, beyond 100 air miles of the farm

**UCR is required for all carriers with interstate DOT numbers.
See page 28 for more information.**

FEDERAL MOTOR CARRIER SAFETY REGULATIONS

The Federal Motor Carrier Safety Regulations are applicable to all motor carriers, drivers, and vehicles that transport property or passengers in commerce when the vehicle:

- Has a GVWR, GCWR, gross vehicle weight, or gross combination weight (whichever is greater) over 10,000 pounds;
- Is designed or used to transport more than 8 passengers (including the driver) for compensation;
- Is designed or used to transport more than 15 passengers, including the driver and is not used to transport passengers for compensation; or
- Is used in transporting material found to be hazardous under 49 U.S.C. 5103 and transported in a quantity requiring placarding under regulations prescribed by the Secretary under 49 CFR, Subtitle B, Chapter I, Subchapter C.

The following chart indicates applicability of the FMCSRs for property-carrying commercial motor vehicles only. For additional Hazardous Materials regulations, see page 46. For Passenger Carrier regulations, see page 44.

49 CFR Part	Topic	Interstate		Intrastate	
		CDL	Non-CDL	CDL	Non-CDL
382	Drug and Alcohol Testing	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
383	Commercial Driver's License	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
385	Safety Fitness Procedures	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
386	Rules of Practice	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
387	Financial Responsibility	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
390	General	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
391	Qualifications of Drivers	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
392	Driving of Commercial Motor Vehicles	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
393	Parts and Accessories	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
395	Hours of Service	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
396	Inspection, Repair, and Maintenance	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
397	Transportation of Hazardous Materials	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Federal Motor Carrier Safety Regulation Parts

FMCSRs can be found online at: www.fmcsa.dot.gov

Part 382 – Controlled Substances and Alcohol Use and Testing: specifies that motor carriers must conduct drug and alcohol testing and includes regulations that prohibit a driver's use and/or possession of alcohol and/or controlled substances.

Part 383 – Commercial Driver's License Standards: specifies when a commercial driver's license (CDL) is required, what type of CDL is required, and what will cause a CDL to be disqualified.

Part 385 – Safety Fitness Procedures: specifies the requirements for Safety Audits, Compliance Reviews, Hazardous Materials Safety Permits, and safety ratings.

Part 386 – Rules of Practice for Proceedings: specifies the requirements proceedings, hearings, decisions, injunctions, imminent hazard orders, and penalties for motor carriers who fail to comply with applicable rules/regulations/laws.

Part 387 – Minimum Levels of Financial Responsibility: specifies when and how much financial responsibility (insurance) is required for a motor carrier's operations.

Part 390 – General: specifies who needs a DOT number, defines common terms, establishes DOT marking requirements, and includes other general regulatory requirements.

Part 391 – Qualifications of Drivers: specifies driver qualification requirements for motor carriers (such as medical certificates, age of driver, record retention requirements, driver's license requirements, etc.).

Part 392 – Driving of Commercial Motor Vehicles: specifies that every commercial motor vehicle must be operated in compliance with the laws, ordinances, and regulations (such as speeding, improper parking, mud-flap requirements, etc.) of the jurisdiction in which it is being operated. This Part also includes regulations regarding texting and the use of other electronic devices such as cell phones.

Part 393 – Parts and Accessories Necessary for Safe Operation: specifies what equipment and accessories (such as lights, brakes, emergency equipment, etc.) are required for commercial motor vehicles and the use of and safe operation of such equipment/accessories.

Part 395 – Hours of Service of Drivers: specifies who is subject to and what the requirements are for hours of service and logbook compliance. Part 395 also addresses automatic on-board recording devices, drivers being placed out-of-service, and exemptions from hours of service and logbook requirements.

Part 396 – Inspection, Repair, and Maintenance: specifies that each motor carrier must systematically inspect (using periodic/annual inspections and daily vehicle inspection reports), repair, and maintain all motor vehicles subject to its control.

Part 397 - Transportation of Hazardous Materials (Driving and Parking Rules)
Part 397 establishes driving and parking requirements for motor carriers and drivers transporting hazardous materials (HM). This Part includes regulations for HM routing, HM vehicle attendance, parking, fueling, tires, fires, and driver smoking.

Markings on Commercial Motor Vehicles

All commercial motor vehicles with a gross vehicle weight rating, gross combination weight rating, gross vehicle weight, or gross combination weight (whichever is greater) over ten thousand pounds must be marked **on the power unit** as specified below

Nature of marking. The marking must display the following information:

- The legal name or a single trade name (DBA) of the motor carrier as listed on the motor carrier identification report (MCS-150 form);
- The motor carrier identification number (USDOT number) issued by the FMCSA, preceded by the letters “USDOT”;
- For intrastate-ONLY carriers: The motor carrier identification number (USDOT number) issued by the FMCSA, preceded by the letters “USDOT” and followed by the letters “NE”;
- If the name of any person other than the operating carrier appears on the commercial motor vehicle (CMV), the name of the operating carrier must be followed by the information required above, and be preceded by the words “operated by”; and
- Other identifying information may be displayed on the CMV if it is not inconsistent with the information required by this section.

Size, shape, location, and color of marking. The marking must:

- Appear on both sides of the power unit;
- Be in letters that contrast sharply in color with the background on which the letters are placed;
- Be readily legible, during daylight hours, from a distance of 50’ while the CMV is stationary; and
- Be kept and maintained in a manner that retains the legibility required.

Construction and durability. The marking may:

- Be painted on the commercial motor vehicle; or
- Consist of a removable device, if that device meets the identification and legibility requirements.

Markings on Commercial Motor Vehicles

(Continued)

Rented commercial motor vehicles: A motor carrier operating a CMV under a rental agreement having a term not in excess of 30 calendar days meets the requirements of this section if:

- The commercial motor vehicle is marked in accordance with the provisions shown above; or
- The commercial motor vehicle is marked as set forth below:
 - The legal name or trade name as well as the USDOT number of the lessor is displayed in accordance with the size, shape, color, construction, and durability requirements shown above; and
 - The rental agreement entered into by the lessor and the renting motor carrier conspicuously contains the following information:
 - The name and complete physical address of the principal place of business of the renting motor carrier;
 - The renting motor carrier's USDOT number, preceded by the letters "USDOT"; and
 - The rental agreement entered into by the lessor and the renting motor carrier is carried on the rental CMV during the full term of the rental agreement.

Driveaway services:

- In driveaway services, a removable device may be affixed on both sides or at the rear of a single driven vehicle.
- In a combination driveaway operation, the device may be affixed on both sides of any one unit or at the rear of the last unit.
- The removable device must display the legal name or single trade name of the motor carrier and the motor carrier's USDOT number.

NOTE: The marking and USDOT number requirements do not apply to motor vehicles registered as Nebraska farm trucks and operated solely in intrastate commerce.

Intrastate DOT application forms are available at:

- www.statepatrol.nebraska.gov/intrastatedotnumber.aspx; or
- For online application: www.fmcsa.dot.gov

Interstate DOT applications are available at: www.fmcsa.dot.gov.

DOT MEDICAL CERTIFICATE (DOT PHYSICAL)

A driver who operates a CDL-requiring vehicle in Nebraska or operates *any* commercial motor vehicle in interstate commerce is required to have a DOT medical certificate.

A driver is exempt from this requirement if he/she is:

- An employee of and driving for a government or political subdivision;
- A custom harvester;
- A beekeeper;
- A driver for certain non-business private motor carriers of passengers;
- Operating farm-plated vehicles solely in Nebraska;
- A driver who has held a Nebraska CDL since before July 30, 1996 and is operating solely in Nebraska; or
- Operating a farm-plated straight truck (not pulling a trailer) within 150 air-miles of the farmer's farm.

DOT Medical Certificates are not required for drivers of non-CDL vehicles that are operated solely in intrastate (Nebraska-only) commerce.

LOGBOOKS

A driver is required to complete and maintain a record of duty status (logbook) when he or she operates a CDL-requiring vehicle in Nebraska or operates *any* commercial motor vehicle in interstate commerce (including pickup trucks, pickup trucks pulling trailers, straight trucks, etc.) There are a few exemptions from the logbook requirements; the most common are summarized below.

A driver of a **CDL-requiring** vehicle is exempt from logbook requirements when he/she meets all of the following requirements. The driver must:

- Operate within 100 air-miles of his/her normal work reporting location;
- Be released from work within 12 hours of when he/she started;
- Operate in compliance with applicable hours of service rules; and
- Complete and retain a time record that includes start time, stop time, total hours, and date for every day an applicable vehicle is driven.

A driver of a **non-CDL-requiring** vehicle is exempt from logbook requirements when he/she meets all of the following requirements. The driver must:

- Operate within 150 air-miles of his/her normal work reporting location;
- Operate in compliance with applicable hours of service rules; and
- Complete and retain a time record that includes start time, stop time, total hours, and date for every day an applicable vehicle is driven.

A driver who transports **agricultural commodities and/or farm supplies** is exempt from logbook and hours of service requirements when operating within 100 air-miles of the source of the commodity or distribution point. This exemption only applies from February 15 through December 15, except when transporting livestock feed, in which case it applies year-round.

HOURS OF SERVICE

Hours of service requirements for drivers of property-carrying commercial motor vehicles:

- **Interstate:**
 - 11 hours maximum driving time
 - Must take 10 consecutive hours off-duty/sleeper berth to start over
 - 14 hours maximum “on-duty” time*
 - Must take 10 consecutive hours off-duty/sleeper berth to start over
 - 60/70 hours maximum combined on-duty and driving time in 7/8 days
 - 34 consecutive hours off-duty/sleeper berth to start over
- **Intrastate:**
 - 12 hours maximum driving time
 - Must take 8 consecutive hours off-duty/sleeper berth to start over
 - 16 hours maximum “on-duty” time*
 - Must take 8 consecutive hours off-duty/sleeper berth to start over
 - 70/80 hours maximum combined on-duty and driving time in 7/8 days
 - 34 consecutive hours off-duty/sleeper berth to start over
- **Changes to the hours of service requirements, effective on July 1, 2013:**
 - Drivers may not drive if more than 8 hours have passed since the end of the driver’s last off-duty or sleeper berth period of at least 30 minutes (must take at least a 1/2 hour break within first 8 hours after coming on-duty)
 - Drivers may not take a 34 hour restart until 168 or more consecutive hours have passed since the beginning of the last 34 hour restart.
 - 34 hour restart must include two periods from 1 a.m. to 5 a.m.

For drivers of passenger-carrying commercial motor vehicles, see the Federal Motor Carrier Safety Regulations (FMCSRs Part 395). The FMCSRs are available at: www.fmcsa.dot.gov.

*14 and 16 hours “on-duty” time refers to all on-duty, off-duty, driving, and sleeper berth periods (excluding sleeper berth periods of 8 consecutive hours or more).

For farm operations, see page 35 for hours of service requirements.

MOTOR CARRIERS OF PASSENGERS

You are a **motor carrier of passengers** if you transport passengers in a vehicle that is designed or used to transport more than:

1. 8 passengers (including the driver) for compensation; or
2. 15 passengers (including the driver).

You are a **for-hire** motor carrier of passengers if you transport passengers for compensation in commerce.

Regulations for For-Hire Motor Carriers of Passengers

If you are a motor carrier that transports passengers for-hire in a vehicle that is designed or used to transport 9 or more passengers (including the driver), you are subject to Federal regulations depending on the number of passengers your vehicle is designed or used to transport and the type of compensation you receive.

49 CFR Part	Topic	9 to 15 passengers for direct compensation**	9 to 15 passengers not for direct compensation	16 or more passengers
382	Drug and Alcohol Testing	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
383	Commercial Driver's License (CDL)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
385	Safety Fitness Procedures	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
387	Financial Responsibility	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
390	General	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
391	Qualifications of Drivers	<input checked="" type="checkbox"/> *	<input type="checkbox"/>	<input checked="" type="checkbox"/>
392	Driving of Commercial Motor Vehicles	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
393	Parts and Accessories	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
395	Hours of Service	<input checked="" type="checkbox"/> *	<input type="checkbox"/>	<input checked="" type="checkbox"/>
396	Inspection, Repair, and Maintenance	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

*Parts 391 and 395 do not apply to intrastate-only motor carriers when operating 9 to 15 passenger vehicles.

****Direct compensation** is payment made to the motor carrier by the passengers or the individual acting on behalf of the passengers for the transportation services provided, and not included in a total package charge or other assessment for highway transportation services.

Private Motor Carriers of Passengers

You are a **private** motor carrier of passengers if you do *not* offer transportation services for -hire, but you do transport passengers in a vehicle either designed to transport more than 15 passengers (including the driver) or with a gross vehicle weight rating or gross vehicle weight of more than 10,000 lbs. There are two types of private motor carriers of passengers: non-business and business.

Non-Business Private Motor Carriers of Passengers

You are a **non-business** private motor carrier of passengers if you are not engaged in a commercial enterprise and provide private transportation of passengers. Examples include churches, scout groups, and other charitable organizations that use buses for the private transportation of their groups.

Business Private Motor Carriers of Passengers

You are a **business** private motor carrier of passengers if you provide transportation of passengers as part of a commercial enterprise but the transportation is not available to the public at large. Examples include companies that use buses to transport their employees and professional musicians who use buses for concert tours.

As a private motor carrier of passengers, you are subject to the following regulations:

49 CFR Part	Topic	Business	Non-Business
382	Drug and Alcohol Testing	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
383	Commercial Driver's License (CDL)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
390	General	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
391	Qualifications of Drivers	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/> *
392	Driving of Commercial Motor Vehicles	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
393	Parts and Accessories	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
395	Hours of Service	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/> *
396	Inspection, Repair, and Maintenance	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/> *

*There are limited exemptions to these Parts for non-business carriers. See www.fmcsa.dot.gov or your copy of the Federal Motor Carrier Safety Regulations for more information.

HAZARDOUS MATERIALS SAFETY REGULATIONS

Hazardous Materials Regulation Parts

The parts, subparts and sections of, Title 49 of the Code of Federal Regulations listed below, or any parts, subparts, and sections referred to by such parts, subparts, and sections, are adopted as Nebraska law and shall be applicable to all motor carriers whether engaged in interstate or intrastate commerce, drivers of such motor carriers, and vehicles of such motor carriers:

- Part 107 – HAZARDOUS MATERIALS PROGRAM PROCEDURES, subpart F – Registration of Cargo Tank and Cargo Tank Motor Vehicle Manufacturers, Assemblers, Repairers, Inspectors, Testers, and Design Certifying Engineers;
- Part 107 – HAZARDOUS MATERIALS PROGRAM PROCEDURES, subpart G – Registration of Persons Who Offer or Transport Hazardous Materials;
- Part 171 – GENERAL INFORMATION, REGULATIONS, AND DEFINITIONS;
- Part 172 – HAZARDOUS MATERIALS TABLE, SPECIAL PROVISIONS, HAZARDOUS MATERIALS COMMUNICATIONS, EMERGENCY RESPONSE INFORMATION, TRAINING REQUIREMENTS AND SECURITY PLANS
- Part 173 – SHIPPERS - GENERAL REQUIREMENTS FOR SHIPMENTS AND PACKAGINGS;
- Part 177 – CARRIAGE BY PUBLIC HIGHWAY
- Part 178 – SPECIFICATIONS FOR PACKAGINGS; and
- Part 180 – CONTINUING QUALIFICATION AND MAINTENANCE OF PACKAGINGS.

Hazardous Materials Transportation

§173.5 Agricultural Operations.

- a) For other than a Class 2 material, the transportation of an agricultural product over local roads between fields of the same farm is excepted from the requirements of this subchapter. A Class 2 material transported over local roads between fields of the same farm is excepted from subparts G and H of part 172 of this subchapter. In either instance, transportation of the hazardous material is subject to the following conditions:
 - i) It is transported by a farmer who is an intrastate private motor carrier; and
 - ii) The movement of the agricultural product conforms to requirements of the State in which it is transported and is specifically authorized by a State statute or regulation in effect before October 1, 1998.
- b) The transportation of an agricultural product to or from a farm, within 150 miles of the farm, is excepted from the requirements in subparts G and H of part 172 of this subchapter and from the specific packaging requirements of this subchapter when:
 - i) It is transported by a farmer who is an intrastate private motor carrier;
 - ii) The total amount of agricultural product being transported on a single motor vehicle does not exceed:
 - A) 7,300 kg (16,094 lbs.) of ammonium nitrate fertilizer properly classed as Division 5.1, PG III, in a bulk packaging, or
 - B) 1900 L (502 gallons) for liquids or gases, or 2,300 kg (5,070 lbs.) for solids, of any other agricultural product;

- iii) The movement and packaging of the agricultural product conform to the requirements of the State in which it is transported and are specifically authorized by a State statute or regulation in effect before October 1, 1998; and
 - iv) Each person having any responsibility for transporting the agricultural product or preparing the agricultural product for shipment has been instructed in the applicable requirements of this subchapter.
- c) Formulated liquid agricultural products in specification packagings of 220 L (58 gallons) capacity, or less, with closures manifolded to a closed mixing system and equipped with positive dry disconnect devices may be transported by a private motor carrier between a final distribution point and an ultimate point of application or for loading aboard an airplane for aerial application.
- d) *Moveable fuel storage tenders.* A non-DOT specification cargo tank motor vehicle may be used to transport Liquefied petroleum gas, UN1075, including Propane, UN1978, as moveable fuel storage tender used exclusively for agricultural purposes when operated by a private carrier under the following conditions:
- 1) The cargo tank must have a minimum design pressure of 250 psig.
 - 2) The cargo tank must meet the requirements of the HMR in effect at the time of its manufacture and must be marked accordingly. For questions regarding these requirements, contact PHMSA by either:
 - A) Telephone (800) 467-4922 or (202) 366-4488 (local); or
 - B) By electronic mail (e-mail) to: *infocntr@dot.gov*.
 - 3) The cargo tank must have a water capacity of 1,200 gallons or less.
 - 4) The cargo tank must conform to applicable requirements in National Fire Protection Association (NFPA) 58, Liquefied Petroleum Gas Code (IBR, see §171.7 of this subchapter).
 - 5) The cargo tank must be securely mounted on a motor vehicle.
 - 6) The cargo tank must be filled in accordance with §173.315(b) for liquefied petroleum gas.
 - 7) The cargo tank must be painted white, aluminum, or other light-reflecting color.
 - 8) Transportation of the filled moveable fuel storage tender is limited to movements over local roads between fields using the shortest practical distance.
 - 9) Transportation of the moveable fuel storage tender between its point of use and a liquefied petroleum gas distribution facility is authorized only if the cargo tank contains no more than five percent of its water capacity. A movable fuel storage tender may only be filled at the consumer's premises or point of use.
- e) *Liquid soil pesticide fumigants.* MC 306 and DOT 406 cargo tank motor vehicles and DOT 57 portable tanks may be used to transport liquid soil pesticide fumigants, Pesticides, liquid, toxic, flammable, n.o.s., *flash point not less than 23 degrees C*, 6.1, UN2903, PG II, exclusively for agricultural operations by a private motor carrier between a bulk loading facility and a farm (including between farms). However, transportation is not to exceed 150 miles between the loading facility and the farm, and not more than five days are permitted for intermediate stops for temporary storage. Additionally, transport is permitted only under the following conditions:
- 1) *Cargo tanks.* MC 306 and DOT 406 cargo tank motor vehicles must:
 - i) Meet qualification and maintenance requirements (including periodic testing and inspection) in accordance with Subpart E of Part 180 of this subchapter;
 - ii) Conform to the pressure relief system requirements specified in §173.243(b) (1);

- iii) For MC 306 cargo tanks, be equipped with stop-valves capable of being remotely closed by manual and mechanical means; and
- iv) For DOT 406 cargo tanks, conform to the bottom outlet requirements specified in §173.243(b)(2).

2) *Portable tanks*. DOT 57 portable tanks must—

- i) Be constructed of stainless steel; and
- ii) Meet qualification and maintenance requirements of Subpart G of Part 180 of this subchapter.

f) See §173.315(m) pertaining to nurse tanks of anhydrous ammonia.

g) See §173.6 pertaining to materials of trade.

h) See §172.800(b) pertaining to security plans.

§173.8 Exceptions for non-specification packagings used in intrastate transportation.

a) *Non-specification bulk packagings*. Notwithstanding requirements for specification packagings in subpart F of this part and parts 178 and 180 of this subchapter, a non-specification bulk packaging may be used for transportation of a hazardous material by an intrastate motor carrier until July 1, 2000, in accordance with the provisions of paragraph (d) of this section.

b) *Non-specification cargo tanks for petroleum products*. Notwithstanding requirements for specification packagings in subpart F of this part and parts 178 and 180 of this subchapter, a non-specification cargo tank motor vehicle having a capacity of less than 13,250 L (3,500 gallons) may be used by an intrastate motor carrier for transportation of a flammable liquid petroleum product in accordance with the provisions of paragraph (d) of this section.

c) *Permanently secured non-bulk tanks for petroleum products*. Notwithstanding requirements for specification packagings in subpart F of this part 173 and parts 178 and 180 of this subchapter, a non-specification metal tank permanently secured to a transport vehicle and protected against leakage or damage in the event of a turnover, having a capacity of less than 450 L (119 gallons), may be used by an intrastate motor carrier for transportation of a flammable liquid petroleum product in accordance with the provisions of paragraph (d) of this section.

d) *Additional requirements*. A packaging used under the provisions of paragraphs (a), (b) or (c) of this section must—

- 1) Be operated by an intrastate motor carrier and in use as a packaging for hazardous material before October 1, 1998;
- 2) Be operated in conformance with the requirements of the State in which it is authorized;
- 3) Be specifically authorized by a State statute or regulation in effect before October 1, 1998, for use as a packaging for the hazardous material being transported;
- 4) Be offered for transportation and transported in conformance with all other applicable requirements of this subchapter;
- 5) Not be used to transport a flammable cryogenic liquid, hazardous substance, hazardous waste, or a marine pollutant (except for gasoline); and
- 6) On and after July 1, 2000, for a tank authorized under paragraph (b) or (c) of this section, conform to all requirements in part 180 (except for §180.405(g)) of this subchapter in the same manner as required for a DOT specification MC 306 cargo tank motor vehicle.

Hazardous Materials Requirements

Shipping Papers - Every shipment of hazardous materials must be accompanied by a properly prepared shipping paper carried in the cab of the power unit, accessible to law enforcement or emergency response personnel. Exceptions: A limited quantity package unless the material is offered or intended for transportation by air or vessel and, until December 31, 2020, a package of ORM-D material authorized by this subchapter in effect on October 1, 2010 when offered for transportation by highway, rail, or vessel and Category B infectious substances prepared in accordance with §173.199. Copies of the hazardous material shipping papers must be retained for 1 year. For a hazardous waste, the shipping paper copy must be retained for 3 years. Refer to 49 CFR 177.817(f) for additional information on the retention of shipping papers.

Marking Requirements - Marking is generally the responsibility of the shipper. Required markings must be legible, in English and printed on or attached to packages or on a label, tag, or sign. They must not be obscured by labels or attachments and must be clear of other markings that might reduce their effectiveness. The proper shipping name and identification number for the hazardous material must be marked on each non-bulk package.

General Labeling Requirements - Labeling is the responsibility of the shipper and carrier. Labels on packages must indicate the nature of the hazard of the contents. Labels must be placed near the marked proper shipping name of the package contents. If multiple labels are required, they must be displayed next to each other. No person may offer for transportation and no carrier may transport a package bearing any marking or label which by its color, design, or shape could be confused with or conflict with a label prescribed by this part.

General Placarding Requirements - Placards must not be displayed on a vehicle unless the vehicle contains hazardous materials and the placard represents a hazard of the material contained.

No placards are required for the following hazardous materials:

- Infectious substances;
- Hazardous materials classed as ORM-D;
- Hazardous materials authorized by this subchapter to be offered for transportation as Limited Quantities when identified as such on shipping papers in accordance with §172.203(b); or when marked as such in accordance with §172.315.
- Hazardous materials prepared in accordance with §173.13 of this subchapter;
- Hazardous materials which are packaged as small quantities under the provisions of §§173.4, 173.4a, 173.4b of this subchapter; and
- Combustible liquids in non-bulk packagings.

General Placarding Requirements (continued)

Placard requirements are governed by the hazard class of the material, quantity, and by certain special requirements as outlined in Federal Hazardous Material Regulations 49 CFR 172.504. When placarding of the vehicle is required, such placarding must be displayed in addition to any other placarding required. Regulations require the shipper furnish the required placards for its shipment prior to, or at the time the vehicle is loaded, unless the motor carriers vehicle is already placarded for the material as required by this subpart.

Placards shall be placed on each side and each end free of accessories and/or appurtenances that would tend to obscure it, and shall be displayed square on point and maintained in a legible condition during transportation.

Hazardous Materials Incidents or Spills - Carriers involved in accidents where hazardous materials, hazardous substances, or hazardous wastes are spilled are required to provide notice to the National Response Center (NRC) by telephone at 800-424-8802 (toll free) or 202-267-2675 (toll call), or online at www.nrc.uscg.mil. A written report will also be required in accordance with §171.16 within 30 days of discovery of an incident.

Railroad Grade Crossing Requirements - In accordance with Nebraska State Statute 60-6,173, drivers of vehicles required to be placarded pursuant to Section 75-364 must stop at railroad crossings, except for those abandoned or marked exempt.

In addition, the Federal Motor Carrier Safety Administration under Title 49 CFR §392.10 regulates railroad grade crossings. Additional information may be found on FMCSA's website: www.fmcsa.dot.gov.

Training Requirements (49 CFR Subchapter C Part 172.704)

(a) Hazmat employee training must include the following:

- (1) **General awareness/familiarization training.** Each hazmat employee shall be provided general awareness/familiarization training designed to provide familiarity with the requirements of this subchapter, and to enable the employee to recognize and identify hazardous materials consistent with the hazard communication standards of this subchapter.
- (2) **Function-specific training.**
 - (i) Each hazmat employee must be provided function-specific training concerning requirements of this subchapter, or exemptions or special permits issued under subchapter A of this chapter, that are specifically applicable to the functions the employee performs.
 - (ii) As an alternative to function-specific training on the requirements of this subchapter, training relating to the requirements of the ICAO Technical Instructions and the IMDG Code may be provided to the extent such training addresses functions authorized by §171.11 and §171.12 of this subchapter.
- (3) **Safety training.** Each hazmat employee shall receive safety training concerning—
 - (i) Emergency response information required by subpart G of part 172;
 - (ii) Measures to protect the employee from the hazards associated with hazardous materials to which they may be exposed in the work place, including specific measures the hazmat employer has implemented to protect employees from exposure; and
 - (iii) Methods and procedures for avoiding accidents, such as the proper procedures for handling packages containing hazardous materials.
- (4) **Security awareness training.** Each hazmat employee must receive training that provides an awareness of security risks associated with hazardous materials transportation and methods designed to enhance transportation security. This training must also include a component covering how to recognize and respond to possible security threats. New hazmat employees must receive the security awareness training required by this paragraph within 90 days after employment.

In-depth security training. Each hazmat employee of a person required to have a security

plan in accordance with subpart I of this part who handles hazardous materials covered by the plan, performs a regulated function related to the hazardous materials covered by the plan, or is responsible for implementing the plan must be trained concerning the security plan and its implementation. Security training must include company security objectives, organizational security structure, specific security procedures, specific security duties and responsibilities for each employee, and specific actions to be taken by each employee in the event of a security breach.

Additional Information on the Hazmat Training Requirements may be found at the Pipeline and Hazardous Materials Safety Administration website www.phmsa.dot.gov/hazmat/training/requirements.

Hazardous Materials Registration Requirements

§ 107.601 Applicability.

- (a) The registration and fee requirements of this subpart apply to any person who offers for transportation, or transports, in foreign, interstate or intrastate commerce—
- (1) A highway route-controlled quantity of a Class 7 (radioactive) material, as defined in §173.403 of this chapter;
 - (2) More than 25 kg (55 pounds) of a Division 1.1, 1.2, or 1.3 (explosive) material (see §173.50 of this chapter) in a motor vehicle, rail car or freight container;
 - (3) More than one L (1.06 quarts) per package of a material extremely toxic by inhalation (*i.e.*, “material poisonous by inhalation,” as defined in §171.8 of this chapter, that meets the criteria for “hazard zone A,” as specified in §§173.116(a) or 173.133(a) of this chapter);
 - (4) A shipment of a quantity of hazardous materials in a bulk packaging (see §171.8 of this chapter) having a capacity equal to or greater than 13,248 L (3,500 gallons) for liquids or gases or more than 13.24 cubic meters (468 cubic feet) for solids;
 - (5) A shipment in other than a bulk packaging of 2,268 kg (5,000 pounds) gross weight or more of one class of hazardous materials for which placarding of a vehicle, rail car, or freight container is required for that class, under the provisions of subpart F of part 172 of this chapter; or
 - (6) Except as provided in paragraph (b) of this section, a quantity of hazardous material that requires placarding, under provisions of subpart F of part 172 of this chapter.
- (b) Paragraph (a)(6) of this section does not apply to those activities of a farmer, as defined in §171.8 of this chapter, that are in direct support of the farmer's farming operations.
- (c) In this subpart, the term “shipment” means the offering or loading of hazardous material at one loading facility using one transport vehicle, or the transport of that transport vehicle.

Additional Information regarding the Hazmat Registration may be found at the Pipeline and Hazardous Materials Safety Administration website at www.phmsa.dot.gov/hazmat/regs.

Hazardous Material Safety Permit (HMSP)

Definition

The Hazardous Materials Safety Permit (HMSP) means a document issued by FMCSA that contains a permit number and confers authority to transport in commerce hazardous materials listed in §385.403.

Materials that require a HM Safety Permit

The following hazardous materials transported in these quantity amounts will require an HM Safety Permit.

- **Radioactive Materials:** A highway route controlled quantity of Class 7 material, as defined in §173.403 of 49 CFR.
- **Explosives** - More than 25kg (55lbs) of a Division 1.1, 1.2 or 1.3 material, or an amount of Division 1.5 materials requiring a placard under Part 172 subpart F of 49 CFR.
- **Toxic by Inhalation Materials**
 - **Hazard Zone A** - A More than one liter (1.08 quarts) per package of a "material poisonous by inhalation," as defined in §171.8 of 49 CFR, that meets the criteria for "Hazard Zone A," as specified in §173.116(a) of §173.133(a) of 49 CFR.
 - **Hazard Zone B** - A "material poisonous by inhalation," as defined in §171.8 of 49 CFR, that meets the criteria for "Hazard Zone B," as specified in §173.116(a) or §173.133(a) of 49 in bulk packaging (capacity greater than 450 L [119 gallons] for liquids and a water capacity greater than 454 kg [1,000 pounds] for gas receptacles.
 - **Hazard Zone C & D** - A "material poisonous by inhalation," as defined in §171.8 of 49 CFR, that meets the criteria for "Hazard Zone C," or Hazard Zone D," as specified in §173.116(a) or §173.133(a) of 49 CFR, in a packaging having a capacity equal to or greater than 13,248L (3,500 gallons).
This includes Anhydrous Ammonia.
- **Methane** - A shipment of compressed or refrigerated liquid methane or liquefied natural gas or other gas with a methane content of at least 85 percent in a bulk packaging having a capacity equal to or greater than 13,248 L (3,500 gallons) for liquids or gases.

To transport hazardous materials listed above, submit form MCS-150B to register for a USDOT number and HMSP at the same time. If you already have a USDOT number, update your MCS-150 registration with the appropriate material(s) to receive an HMSP. Additional information on applying for a Hazardous Materials Safety Permit may be found on FMCSA's website: www.fmcsa.dot.gov.

WEIGH IN MOTION AND PREPASS SYSTEMS

Weigh in Motion (WIM)

All Nebraska Interstate scale facilities and the Nebraska City scale facility utilize Weigh-In-Motion (WIM) which is a vehicle sorting system. This system weighs the vehicle approximately ¼ mile prior to the scale facility and determines if the weight of the vehicle is within a specified range of legal weights. The vehicle in question will get one of two messages on the message boards. One message advises the driver that the vehicle is okay to bypass, the other message advises the driver that the vehicle must exit into the scale facility.

Any vehicle operating under a Nebraska Department of Roads overweight/over dimensional permit is required to stop at all open scale facilities.

PrePass

All Nebraska Interstate scale facilities and the Nebraska City scale facility utilize the PrePass system, which is a vehicle sorting system. Companies who register with PrePass for this service are issued a transponder for each vehicle entered into the system. When one of these vehicles approaches the facility the driver of the vehicle will receive one of two signals on the transponder; a green light indicating that the vehicle is okay to bypass the scale facility or a red light indicating that the vehicle must stop at the scale facility. Drivers are instructed to always follow the PrePass in cab signal, regardless of what the Weigh-In-Motion message boards direct them to do.

Any vehicle operating under a Nebraska Department of Roads overweight/over dimensional permit is required to stop at all open scale facilities.

The state of Nebraska *does not* issue PrePass.

For more information on the PrePass system go to www.prepass.com.

Fees for use will apply.

GLOSSARY OF TERMS

Axle Load (Single Axle): the total load transmitted to the highway by all wheels the centers of which may be included between two parallel transverse vertical planes 40" apart extending across the full width of the vehicle.

Commercial Enterprise: any business activity relating to or based upon the production, distribution, or consumption of goods or services.

Commercial Motor Vehicle (CMV) (For Purposes of Commercial Drivers License): a motor vehicle or combination of motor vehicles used in commerce to transport passengers or property if the motor vehicle:

- (a) Has a GVWR of 26,001 pounds or more inclusive of towed units with a GVWR of more than 10,000 pounds;
- (b) has a GVWR of 26,001 pounds or more;
- (c) Is designed to transport 16 or more passengers, including the driver; or
- (d) Is used in the transportation of materials found to be hazardous for the purposes of the Federal Hazardous Materials Transportation Act and which require the motor vehicle to be placarded under 49 CFR part 172, subpart F.

Commercial motor vehicle does not include:

- (a) a farm vehicle as defined in section 60-325 other than a combination of truck-tractors and semi trailers when such farm vehicle is operated within 150 miles of the owner's farm or ranch,
- (b) any recreational vehicle as defined in section 60-347 or motor vehicle towing a cabin trailer as defined in sections 60-314 and 60-339,
- (c) any emergency vehicle operated by a public or volunteer fire department, or
- (d) any motor vehicle owned or operated by the United States Department of Defense or Nebraska National Guard when such motor vehicle is driven by persons identified in section 60-4,131.01.

Commercial Motor Vehicle (CMV) (For Purposes of the Federal Regulations): any self-propelled or towed motor vehicle used on a highway in interstate commerce or intrastate commerce to transport passengers or property when the vehicle:

- (a) Has a GVWR or GCWR or gross vehicle weight or gross combination weight of 10,001 pounds or more, whichever is greater;
- (b) Is designed or used to transport more than 8 passengers, including the driver, for compensation;
- (c) Is designed or used to transport more than 15 passengers, including the driver, and is not used to transport passengers for compensation; or
- (d) Is used in transporting material found to be hazardous and such material is transported in a quantity requiring placarding pursuant to section 75-364.

Dummy Axle: an axle attached to a vehicle or vehicle combination in a manner so that it does not articulate or substantially equalize the load and does not carry at least the lesser of 8,000 pounds or 8% of the gross weight of the vehicle or vehicle combination, including the load.

Farmer: any person who operates a farm or is directly involved in the cultivation of land, crops, or livestock which:

- (a) Are owned by that person; or
- (b) Are under the direct control of that person.

Farm Truck: a truck or sport utility vehicle, including any combination of a truck, truck-tractor, or sport utility vehicle and a trailer or semi trailer, of a farmer or rancher:

- (a) Used exclusively to carry a farmer's or rancher's own supplies, farm equipment, and household goods to or from the owner's farm or ranch;
- (b) Used by the farmer or rancher to carry his or her own agricultural products to or from storage or market;
- (c) Used by a farmer or rancher in exchange of services in such hauling of supplies or agricultural products; or
- (d) Used occasionally to carry camper units, to tow boats or cabin trailers, or to carry or tow museum pieces or historical vehicles, without compensation, to events for public display or educational purposes.

Fertilizer and agricultural chemical application and distribution equipment means:

- (a) Self-propelled or towed equipment, designed and used exclusively to apply commercial fertilizer, as that term is defined in section 81-2,162.02, chemicals, or related products to agricultural soil and crops; or
- (b) Towed equipment designed and used exclusively to carry commercial fertilizer, as that term is defined in section 81-2,162.02, chemicals, or related products for use on agricultural soil and crops, which are equipped with implement or floatation tires.

For-Hire Motor Carrier: a person engaged in the transportation of goods or passengers for compensation.

Full Trailer: any vehicle, with or without motive power, designed for carrying persons or property and for being drawn by a motor vehicle and so constructed that no part of its weight rests upon the towing vehicle.

Gross Combination Weight Rating (GCWR): the value specified by the manufacturer as the loaded weight of a combination (articulated) motor vehicle. In the absence of a value specified by the manufacturer, GCWR will be determined by adding either the GVWR or gross vehicle weight of the motor vehicle plus the GVWR or gross vehicle weight of the towed unit or units.

Gross Vehicle Weight Rating (GVWR): the value specified by the manufacturer as the loaded weight of a single motor vehicle. In the absence of such value specified by the manufacturer or the absence of any marking of such value on the vehicle, the gross GVWR shall be determined from the sum of the axle weight ratings of the vehicle or the sum of the tire weight ratings as marked on the sidewall of the tires, whichever is greater. In the absence of any tire sidewall marking, the tire weight ratings shall be determined for the specified tires from any of the publications of any of the organizations listed in 49 CFR 571.119.

Highway: the entire width between the boundary limits of any street, road, avenue, boulevard, or way which is publicly maintained when any part thereof is open to the use of the public for purposes of vehicular travel.

Idle Reduction Technology (Auxiliary Power Unit or APU): any device or system of devices that is installed on a heavy-duty diesel-powered on-highway truck or truck-tractor and is designed to provide to such truck or truck-tractor those services, such as heat, air conditioning, or electricity, that would otherwise require the operation of the main drive engine while the truck or truck-tractor is temporarily parked or remains stationary.

Implement Of Husbandry: every vehicle or implement designed and adapted exclusively for agricultural, horticultural, or livestock-raising operations or for lifting or carrying an implement of husbandry and used primarily off any highway.

Interstate Commerce: trade, traffic, or transportation provided in the furtherance of a commercial enterprise in the United States:

- (a) Between a place in a state and a place outside of such state, including a place outside of the United States;
- (b) Between two places in a state through another state or a place outside of the United States; or
- (c) Between two places in a state as part of trade, traffic, or transportation originating or terminating outside the state or the United States.

Intrastate Commerce: any trade, traffic, or transportation provided in the furtherance of a commercial enterprise between any place in the State of Nebraska and any other place in Nebraska and not through any other state.

Motor Carrier: a for-hire motor carrier or a private motor carrier. The term includes a motor carrier's agents, officers, and representatives as well as employees responsible for hiring, supervising, training, assigning, or dispatching of drivers and employees concerned with the installation, inspection, and maintenance of motor vehicle equipment or accessories. This definition includes the terms employer and exempt motor carrier.

Motor Vehicle (For purposes of the Federal Regulations): any vehicle, truck, truck-tractor, trailer, or semi trailer propelled or drawn by mechanical power except:

- (a) Farm tractors;
- (b) Vehicles which run only on rails or tracks, or
- (c) Road and general-purpose construction and maintenance machinery which by design and function is obviously not intended for use on a public highway, including, but not limited to, motor scrapers, earthmoving equipment, backhoes, trenchers, motor graders, compactors, tractors, bulldozers, bucket loaders, ditch digging apparatus, asphalt spreaders, leveling graders, power shovels, and crawler tractors.

Motor Vehicle (for the purposes of Nebraska Registration): any vehicle propelled by any power other than muscular power except:

- (a) Mopeds;
- (b) Farm tractors;
- (c) Self-propelled equipment designed and used exclusively to carry and apply fertilizer, chemicals, or related products to agricultural soil and crops, agricultural floater-spreader implements, and other implements of husbandry designed for and used primarily for tilling the soil and harvesting crops or feeding livestock;
- (d) Power unit hay grinders or a combination which includes a power unit and a hay grinder when operated without cargo;
- (e) Vehicles which run only on rails or tracks;
- (f) Off-road designed vehicles, including, but not limited to, golf carts, go-carts, riding lawnmowers, garden tractors, all-terrain vehicles, snowmobiles registered or exempt from registration under sections 60-3,207 to 60-3,219, and minibikes;
- (g) Road and general-purpose construction and maintenance machinery not designed or used primarily for the transportation of persons or property, including, but not limited to, ditch digging apparatus, asphalt spreaders, bucket loaders, leveling graders, earthmoving carryalls, power shovels, earthmoving equipment, and crawler tractors;
- (h) Self-propelled chairs used by persons who are disabled;
- (i) Electric personal assistive mobility devices; and
- (j) Low-speed vehicles.

Out-of-Service Order: a declaration by an authorized enforcement officer of a federal, state, Canadian, Mexican, or local jurisdiction that a driver, a commercial motor vehicle, or a motor carrier operation is out of service pursuant to 49 CFR 386.72, 392.5, 395.13, or 396.9, or compatible laws or the North American Uniform Out-of-Service Criteria.

Private Motor Carrier: a person who provides transportation of property or passengers by commercial motor vehicle and is not a for-hire motor carrier.

Semitrailer: any vehicle, with or without motive power, designed to carry persons or property and to be drawn by a motor vehicle and so constructed that some part of its weight and that of its load rests upon or is carried by another vehicle.

Stinger Steer: a fifth wheel automobile transport located on a drop frame behind and below the rear axle(s) of the power unit with a power unit capable of carrying several vehicles over and behind it.

Tandem Axle: any two consecutive axles the centers of which are more than 40 inches and not more than 96" apart, measured to the nearest inch between any 2 adjacent axles in the series, and the gross weight transmitted to the road surface through such series shall not exceed 34,000 pounds. No axle of the series shall exceed the maximum weight permitted for a single axle.

Tank Vehicle: any commercial motor vehicle that is designed to transport any liquid or gaseous materials within a tank that is either permanently or temporarily attached to the vehicle or chassis. Such vehicle includes, but is not limited to, a cargo tank and a portable tank as defined in 49 CFR 171. Does not include a vehicle hauling a portable tank that has

a rated capacity under 1,000 gallons. A commercial motor vehicle hauling a portable tank with a maximum capacity greater than 119 gallons that is not permanently attached to the vehicle, but is loaded and unloaded without being removed from the vehicle is to be treated the same as if it were a permanently attached cargo tank.

Truck: a motor vehicle that is designed, used, or maintained primarily for the transportation of property or designated as a truck by the manufacturer.

Truck-tractor: any motor vehicle designed and used primarily for towing other motor vehicles or trailers and not so constructed as to carry a load other than a part of the weight of the motor vehicle or trailer and load being towed.

Utility Trailer: a trailer having a gross weight, including load thereon, of 9,000 lbs. or less.

GLOSSARY OF HAZARDOUS MATERIALS TERMS

Agricultural product means a hazardous material, other than a hazardous waste, whose end use directly supports the production of an agricultural commodity including, but not limited to a fertilizer, pesticide, soil amendment or fuel. An agricultural product is limited to a material in Class 3, 8 or 9, Division 2.1, 2.2, 5.1, or 6.1, or an ORM-D material.

Bulk packaging means a packaging, other than a vessel or a barge, including a transport vehicle or freight container, in which hazardous materials are loaded with no intermediate form of containment. A Large Packaging in which hazardous materials are loaded with an intermediate form of containment, such as one or more articles or inner packagings, is also a bulk packaging. Additionally, a bulk packaging has: * * *

- (1) A maximum capacity greater than 450 L (119 gallons) as a receptacle for a liquid;
- (2) A maximum net mass greater than 400 kg (882 pounds) and a maximum capacity greater than 450 L (119 gallons) as a receptacle for a solid; or
- (3) A water capacity greater than 454 kg (1000 pounds) as a receptacle for a gas as defined in §173.115 of this subchapter.

Cargo tank means a bulk packaging that:

- (1) Is a tank intended primarily for the carriage of liquids or gases and includes appurtenances, reinforcements, fittings, and closures (for the definition of a tank, *see* 49 CFR 178.320, 178.337–1, or 178.338–1, as applicable);
- (2) Is permanently attached to or forms a part of a motor vehicle, or is not permanently attached to a motor vehicle but which, by reason of its size, construction or attachment to a motor vehicle is loaded or unloaded without being removed from the motor vehicle; and
- (3) Is not fabricated under a specification for cylinders, intermediate bulk containers, multi-unit tank car tanks, portable tanks, or tank cars.

Cargo tank motor vehicle means a motor vehicle with one or more cargo tanks permanently attached to or forming an integral part of the motor vehicle.

Combustible liquid means any liquid that does not meet the definition of any other hazard class specified in this subchapter and has a flash point above 60 °C (140 °F) and below 93 °C (200 °F).***

Division means a subdivision of a hazard class.

Farmer means a person engaged in the production or raising of crops, poultry, or livestock.

Flammable liquid. (Class 3) means a liquid having a flash point of not more than 60 °C (140 °F), or any material in a liquid phase with a flash point at or above 37.8 °C (100 °F) that is intentionally heated and offered for transportation or transported at or above its flash point in a bulk packaging, ***

Flash point means the minimum temperature at which a liquid gives off vapor within a test vessel in sufficient concentration to form an ignitable mixture with air near the surface of the liquid.

HMR means the Hazardous Materials Regulations, Parts 171 through 180 of this chapter.

Hazard class means the category of hazard assigned to a hazardous material under the definitional criteria of part 173 of this subchapter and the provisions of the §172.101 table. A material may meet the defining criteria for more than one hazard class but is assigned to only one hazard class.

Hazardous material means a substance or material that the Secretary of Transportation has determined is capable of posing an unreasonable risk to health, safety, and property when transported in commerce, and has designated as hazardous under section 5103 of Federal hazardous materials transportation law (49 U.S.C. 5103). The term includes hazardous substances, hazardous wastes, marine pollutants, elevated temperature materials, materials designated as hazardous in the Hazardous Materials Table (see 49 CFR 172.101), and materials that meet the defining criteria for hazard classes and divisions in part 173 of subchapter C of this chapter.

Hazardous substance for the purposes of this subchapter, means a material, including its mixtures and solutions, that—

- (1) Is listed in the appendix A to §172.101 of this subchapter;
- (2) Is in a quantity, in one package, which equals or exceeds the reportable quantity (RQ) listed in the appendix A to §172.101 of this subchapter; and
- (3) When in a mixture or solution—
 - (i) For radionuclides, conforms to paragraph 7 of the appendix A to §172.101.
 - (ii) For other than radionuclides, is in a concentration by weight which equals or exceeds the concentration corresponding to the RQ of the material, as shown in the following table:

RQ pounds (kilograms)	Concentration by weight	
	Percent	PPM
5000 (2270)	10	100,000
1000 (454)	2	20,000
100 (45.4)	0.2	2,000
10 (4.54)	0.02	200
1 (0.454)	0.002	20

The term does not include petroleum, including crude oil or any fraction thereof which is not otherwise specifically listed or designated as a hazardous substance in appendix A to §172.101 of this subchapter, and the term does not include natural gas, natural gas liquids, liquefied natural gas, or synthetic gas usable for fuel (or mixtures of natural gas and such synthetic gas).

Hazardous waste, for the purposes of this chapter, means any material that is subject to the Hazardous Waste Manifest Requirements of the U.S. Environmental Protection Agency specified in 40 CFR part 262.

Hazmat employee means:

- (1) A person who is:
 - (i) Employed on a full-time, part time, or temporary basis by a hazmat employer and who in the course of such full time, part time or temporary employment directly affects hazardous materials transportation safety;
 - (ii) Self-employed (including an owner-operator of a motor vehicle, vessel, or aircraft) transporting hazardous materials in commerce who in the course of such self-employment directly affects hazardous materials transportation safety;
 - (iii) A railroad signalman; or
 - (iv) A railroad maintenance-of-way employee.
- (2) This term includes an individual, employed on a full time, part time, or temporary basis by a hazmat employer, or who is self-employed, who during the course of employment:
 - (i) Loads, unloads, or handles hazardous materials;
 - (ii) Designs, manufactures, fabricates, inspects, marks, maintains, reconditions, repairs, or tests a package, container or packaging component that is represented, marked, certified, or sold as qualified for use in transporting hazardous material in commerce.
 - (iii) Prepares hazardous materials for transportation;
 - (iv) Is responsible for safety of transporting hazardous materials;
 - (v) Operates a vehicle used to transport hazardous materials.

Hazmat employer means:

- (1) A person who employs or uses at least one hazmat employee on a full-time, part time, or temporary basis; and who:
 - (i) Transports hazardous materials in commerce;
 - (ii) Causes hazardous materials to be transported in commerce; or
 - (iii) Designs, manufactures, fabricates, inspects, marks, maintains, reconditions, repairs or tests a package, container, or packaging component that is represented, marked, certified, or sold by that person as qualified for use in transporting hazardous materials in commerce;
- (2) A person who is self-employed (including an owner-operator of a motor vehicle, vessel, or aircraft) transporting materials in commerce; and who:
 - (i) Transports hazardous materials in commerce;
 - (ii) Causes hazardous materials to be transported in commerce; or
 - (iii) Designs, manufactures, fabricates, inspects, marks, maintains, reconditions, repairs or tests a package, container, or packaging component that is represented, marked, certified, or sold by that person as qualified for use in transporting hazardous materials in commerce; or
- (3) A department, agency, or instrumentality of the United States Government, or an authority of a State, political subdivision of a State, or an Indian tribe; and who:
 - (i) Transports hazardous materials in commerce;
 - (ii) Causes hazardous materials to be transported in commerce; or
 - (iii) Designs, manufactures, fabricates, inspects, marks, maintains, reconditions, repairs or tests a package, container, or packaging component that is represented, marked, certified, or sold by that person as qualified for use in transporting hazardous materials in commerce.

Large packaging means a packaging that—

- (1) Consists of an outer packaging that contains articles or inner packaging;
- (2) Is designated for mechanical handling;
- (3) Exceeds 400 kg net mass or 450 liters (118.9 gallons) capacity;
- (4) Has a volume of not more than 3 cubic meters (m³) (see §178.801(i) of this subchapter); and
- (5) Conforms to the requirements for the construction, testing and marking of Large Packagings as specified in subparts P and Q of part 178 of this subchapter.

Limited quantity, when specified as such in a section applicable to a particular material, means the maximum amount of a hazardous material for which there is a specific labeling or packaging exception.

Marine pollutant, means a material which is listed in appendix B to §172.101 of this subchapter (also see §171.4) and, when in a solution or mixture of one or more marine pollutants, is packaged in a concentration which equals or exceeds:

- (1) Ten percent by weight of the solution or mixture for materials listed in the appendix; or
- (2) One percent by weight of the solution or mixture for materials that are identified as severe marine pollutants in the appendix.

Material of trade means a hazardous material, other than a hazardous waste, that is carried on a motor vehicle—

- (1) For the purpose of protecting the health and safety of the motor vehicle operator or passengers;
- (2) For the purpose of supporting the operation or maintenance of a motor vehicle (including its auxiliary equipment); or
- (3) By a private motor carrier (including vehicles operated by a rail carrier) in direct support of a principal business that is other than transportation by motor vehicle.

Material poisonous by inhalation or Material toxic by inhalation means:

- (1) A gas meeting the defining criteria in §173.115(c) of this subchapter and assigned to Hazard Zone A, B, C, or D in accordance with §173.116(a) of this subchapter;
- (2) A liquid (other than as a mist) meeting the defining criteria in §173.132(a)(1)(iii) of this subchapter and assigned to Hazard Zone A or B in accordance with §173.133(a) of this subchapter; or
- (3) Any material identified as an inhalation hazard by a special provision in column 7 of the §172.101 table.

Non-bulk packaging means a packaging which has:

- (1) A maximum capacity of 450 L (119 gallons) or less as a receptacle for a liquid;
- (2) A maximum net mass of 400 kg (882 pounds) or less and a maximum capacity of 450 L (119 gallons) or less as a receptacle for a solid; or
- (3) A water capacity of 454 kg (1000 pounds) or less as a receptacle for a gas as defined in §173.115 of this subchapter.

ORM–D material - Until December 31, 2020 and for the purposes of this subchapter, “ORM–D material” means a material such as a consumer commodity, cartridges, small arms or cartridges, power devices which, although otherwise subject to the regulations of

this subchapter, presents a limited hazard during transportation due to its form, quantity and packaging. It must be a material for which exceptions are provided in Column (8A) of the §172.101 Hazardous Materials Table.

Packaging means a receptacle and any other components or materials necessary for the receptacle to perform its containment function in conformance with the minimum packing requirements of this subchapter. For radioactive materials packaging, see §173.403 of this subchapter.

PHMSA means the Pipeline and Hazardous Materials Safety Administration, U.S. Department of Transportation, Washington, DC 20590.

Portable tank means a bulk packaging (except a cylinder having a water capacity of 1000 pounds or less) designed primarily to be loaded onto, or on, or temporarily attached to a transport vehicle or ship and equipped with skids, mountings, or accessories to facilitate handling of the tank by mechanical means. It does not include a cargo tank, tank car, multi-unit tank car tank, or trailer carrying 3AX, 3AAX, or 3T cylinders.

Shipping paper means a shipping order, bill of lading, manifest or other shipping document serving a similar purpose and prepared in accordance with subpart C of part 172 of this chapter.

Transport vehicle means a cargo-carrying vehicle such as an automobile, van, tractor, truck, semitrailer, tank car or rail car used for the transportation of cargo by any mode. Each cargo-carrying body (trailer, rail car, etc.) is a separate transport vehicle.

Additional definitions may be found at the Pipeline and Hazardous Materials Safety Administration website at www.phmsa.dot.gov/hazmat/regs.

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